

City of Cincinnati

801 Plum Street Cincinnati, OH 45202

Agenda - Final-revised

Equitable Growth & Housing

Chairperson, Mark Jeffreys
Vice Chair, Victoria Parks
Councilmember, Meeka Owens
Councilmember, Jeff Cramerding
Councilmember, Anna Albi
Vice Mayor, Jan-Michele Kearney
Councilmember, Scotty Johnson
Councilmember, Seth Walsh
Councilmember Evan Nolan

Tuesday, May 6, 2025

1:00 PM

Council Chambers, Room 300

PUBLIC HEARING

PRESENTATIONS

Community Efforts in West End and City of Cincinnati

Kate Solomon, FC Cincinnati

Proposed Zone Change to Planned Development (PD) at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn

Kyle Gibbs, City Planner

AGENDA

1. <u>202500964</u> **PRESENTATION**, submitted by Councilmember Jeffreys from Kate Solomon

regarding Community Efforts in the West End and City of Cincinnati.

Attachments: PRESENTATION

START OF PUBLIC HEARING

2. 202500754 ORDINANCE submitted by Sheryl M. M. Long, City Manager, on 4/16/2025,

AMENDING the official zoning map of the City of Cincinnati to rezone the real property located at 198 E. McMillan Street and 237 William H. Taft Road in the Mt. Auburn neighborhood from the OG-T, "Office General-Transportation," zoning district to Planned Development District No. 104, "Chick-fil-A."

Sponsors: City Manager

Attachments: Transmittal

Ordinance

Legislative Record

Memo to Clerk

3. 202500932 PRESENTATION, submitted by Sheryl M. M. Long, City Manager, dated

5/6/2025, regarding the Ordinance for the Rezoning of 198 E. McMillian Street

and 237 William H Taft Road in Mt. Auburn.

<u>Sponsors:</u> City Manager

<u>Attachments:</u> <u>Transmittal</u>

Presentation

END OF PUBLIC HEARING

ADJOURNMENT



COMMUNITY EFFORTS IN WEST END & CITY OF CINCINNATI



The 10th anniversary of FC Cincinnati is a celebration of our **commitment to excellence – in sport, culture and community. We remain committed to giving back and lifting others** up as we continue to build a world-class club for and by the fans.

Sport: We have met and surpassed challenges on the pitch. We have fostered growth and success in top talent, hoisted trophies and remain laser focused on bringing home MLS Cup.

Culture: Grounded in our values, devoted to our mission, and dedicated to driving impact, we forge ahead as a team of talented professionals committed to leaving a lasting legacy and being the embodiment of a rising Cincinnati.

Community: We strive to unite our community both on and off the pitch, supporting transformational change in the lives of children and those in the West End Community.

We have achieved a great deal but still have so much ground to cover – more championship soccer to play, more room to grow, more opportunities to connect and catalyze change in our community. We've only just begun!

COMMUNITY SUPPORT

- FC Cincinnati Foundation's mission is to improve the lives of children in our region through soccer. We are proud to have reached over 60,000 children in 2025.
- Our CBA with the West End has guided much of our work through FC Cincinnati Foundation.
- We are focused on actively driving community impact and therefore invest most of our funds into programming and in-kind support rather than monetary sponsorship of other events.
- Per our CBA Agreement, we have a Grants Program available which is focused on supporting community building initiatives within the West End Neighborhood.
- We are often able bring third party opportunities to the community as we saw recently with the FIFA visit.







WEST END GRANT PROGRAM

FC Cincinnati Foundation is pleased to support community building initiatives within the West End neighborhood through our grant program.

Applications are accepted during two periods each year.

Organizations selected from applications received March 1April 30 will receive funding in June.

Organizations selected from applications received September 1-October 31 will receive funding in December.

We encourage all applicants to meet with the West End Community Council for their endorsement.

A minimum of \$100,000 in funding is awarded on an annual basis.

BFC CINCINNATION 2024 IN THE COMMUNITY

60,000+ CHILDREN supported in our community

AVERAGED OVER ONE COMMUNITY VISIT A DAY BY AN FCC PLAYER OR STAFF MEMBER

FCC STAFF COMPLETED 3,433 VOLUNTEER HOURS

GENERATING IMPACT THROUGH UNIQUE EXPERIENCES

650 ATTENDEES

AT SERVED BY THE PROS

125 ATTENDEES

AT LADIES LACE UP

\$375,000 raised to support programs



10 MINI PITCHES provide safe access for play



836 HOURS of youth soccer programming



24,000+ REWARDS provided to kids through Learning is Cool



51,954 STUDENTS enrolled in Stay in the Game to improve chronic absenteeism



\$100,000 IN GRANT FUNDING

to West End communitybuilding initiatives



1,767 PARTICIPANTS

in Youth Soccer Camps

☆20%

SOCCER UNITES VISITS

15%

WEST END PRIDE PLAYERS

10%

YOUTH SOCCER
CAMP ATTENDEES

125%

STREET TEAM
COMMUNITY VISITS

COMMITTED TO CELEBRATING WITH OUR COMMUNITY

Black History Month • D.A.D. Initiative Event • Breaking Barriers Event
Asian American and Pacific Islander Month • AAPI Matchday Celebration

Juneteenth Block Party • Juneteenth Parade • Juneteenth Matchday Celebration

Pride Parade • Pride Matchday Celebration

Hispanic Heritage Month • Noche Latina Matchday Celebration

Kick Childhood Cancer Month • Kick Childhood Cancer Matchday Awareness

Salute to Service Matchday Recognition • Veteran Home Improvement Project

CONTINUOUS IMPORVEMENT, GROWTH & ATTENTION TO COMMUNITY NEEDS

- Annual effort for continuous improvement of our programs and to extend our reach to more kids in our community, aided by data including Hello Insights (SEL)
- New programs launched since 2024 include:
 - Stay in the Game!
 - Twinning Project
 - Scholarships with the University of Cincinnati





ALIGNMENT WITH ACT FOR CINCY

Education Programs

- Learning is Cool (Honor Roll Rewards)
- Stay in the Game! (Attendance)
- FCC Futures (Career Pathing through Sport)

Soccer Programs

- West End Pride (Rec Level Soccer)
- Soccer Unites In-School & After-School
- Mini Pitch Locations (7 CRC Sites)





THANK YOU





April 16, 2025

To: Mayor and Members of City Council

202500754

From: Sheryl M. M. Long, City Manager

Subject: Ordinance – Zone Change from Office General-Transportation Corridor (OG-T) to

Planned Development (PD), including a Concept Plan and Development Program Statement, at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn.

Transmitted is an Ordinance captioned:

AMENDING the official zoning map of the City of Cincinnati to rezone the real property located at 198 E. McMillan Street and 237 William H. Taft Road in the Mt. Auburn neighborhood from the OG-T, "Office General-Transportation," zoning district to Planned Development District No. 104, "Chick-fil-A".

The City Planning Commission recommended approval of the designation at its April 4, 2025, meeting.

Summary

The petitioner, Union on Taft LLC, has requested a Zone Change from Office General – Transportation Corridor (OG-T) to Planned Development No. 104 (PD-104) to facilitate a mixed-used development on the property. The Planned Development will include an existing multi-family apartment building and a new restaurant with drive-through service. The site is within the Mt. Auburn neighborhood.

If approved, the proposed mixed-use development would include 103-units of student housing with 180 parking spaces and an outdoor courtyard, and a new Chick-fil-A restaurant with 76 parking spaces, a two-lane drive-though, and seating for 94 guests. The new restaurant is proposed to be a standalone building, approximately 5,020 square feet and 21 feet tall. The proposal includes pedestrian walkways connecting to adjacent sidewalks with internal crosswalks connecting the walkways to the building's entrance.

The City Planning Commission recommended the following on April 4, 2025, to City Council:

APPROVE the Concept Plan and Development Program Statement as submitted; and

ADOPT the proposed zone change from Office General-Transportation (OG-T) to Planned Development (PD) at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn.

cc: Katherine Keough-Jurs, FAICP, Director, Department of City Planning and Engagement

AMENDING the official zoning map of the City of Cincinnati to rezone the real property located at 198 E. McMillan Street and 237 William H. Taft Road in the Mt. Auburn neighborhood from the OG-T, "Office General-Transportation," zoning district to Planned Development District No. 104, "Chick-fil-A."

WHEREAS, Union on Taft, LLC ("Owner") owns the real property located at 198 E. McMillan Street and 237 William H. Taft Road ("Property") in the Mt. Auburn neighborhood, which comprises approximately 5.5 acres and is located in the OG-T, "Office General-Transportation," zoning district; and

WHEREAS, the Property currently consists of a four-story multi-family residential apartment building with 103 units, a large surface parking lot, and greenspace; and

WHEREAS, the Owner has petitioned the City to rezone the Property from the OG-T, "Office General-Transportation," zoning district to Planned Development District No. 104 ("PD-104") to facilitate the construction of a new drive-through restaurant ("Project"); and

WHEREAS, the Project includes maintaining the existing multi-family residential building on the Property and converting the surface parking facility into a drive-through restaurant in the southeast corner of the Property with seating for up to ninety guests including dedicated outdoor seating, approximately 76 parking spaces, landscaping, and new signage; and

WHEREAS, the Owner has submitted a concept plan and development program statement for the Project, which concept plan and development program statement describe the land use and development regulations that will govern the Project and which documents otherwise meet the requirements of Cincinnati Municipal Code Section 1429-09; and

WHEREAS, the Owner has sufficient control over the Property to affect its proposed plan and construct the Project; and

WHEREAS, the Project is compatible with surrounding land uses, will create employment opportunities for the community, convert a surface parking facility into a more productive land use, and will provide additional dining options for nearby residents and attract new visitors to the Mt. Auburn neighborhood; and

WHEREAS, the proposed zone change is in accordance with the "Compete" Initiative Area of Plan Cincinnati (2012), which includes the strategy to "[t]arget investment to geographic areas where there is already economic activity" (page 115), and the action step to "[f]ocus development on the existing centers of activity" (page 116); and

WHEREAS, the proposed rezoning is in accordance with the Mt. Auburn Community Plan (1992), which includes the goal to "[l]imit retail business development to centralized locations near transit and residences" (page 46); and

WHEREAS, on April 4, 2025, the City Planning Commission approved the rezoning of the Property from the OG-T, "Office General-Transportation," zoning district to PD-104, upon a finding that: (i) the Owner's concept plan and development program statement are consistent with applicable plans and policies and are compatible with surrounding development; (ii) the concept plan and development program statement enhance the potential for superior urban design in comparison with the development under the base district regulations that would apply if the plan were not approved; (iii) deviations from the base district regulations applicable to the Property at the time of the Owner's application are justified by compensating benefits of the concept plan and development program statement; and (iv) the concept plan and development program statement include adequate provisions for utility services, refuse collection, open space, landscaping and buffering, pedestrian circulation, traffic circulation, building design, and building location; and

WHEREAS, a committee of Council held a public hearing on the proposed rezoning of the Property following due and proper notice pursuant to Cincinnati Municipal Code Section 111-1, and the committee approved the rezoning, finding it in the interest of the general public's health, safety, morals, and welfare; and

WHEREAS, the Council resolves to rezone the Property from the OG-T, "Office General-Transportation," zoning district to PD-104 finding it to be in the interest of the public's health, safety, morals, and general welfare; now, therefore,

BE IT ORDAINED by the Council of the City of Cincinnati, State of Ohio:

Section 1. That Council hereby adopts as its own independent findings the Cincinnati Planning Commission's findings that the planned development proposed by Union on Taft, LLC for the real property generally located at 198 E. McMillan Street and 237 William H. Taft Road ("Property") in the Mt. Auburn neighborhood as shown on the map attached hereto as Attachment A and incorporated herein by reference conforms to the requirements of Cincinnati Municipal Code Sections 1429-05, 1429-09, and 1429-11.

Section 2. That the shape and area of the City's official zoning map in the location of the Property, which real property is identified on the map attached hereto as Attachment A, and which real property is more particularly described on Attachment B, attached hereto and made a part hereof, is hereby amended from the OG-T, "Office General-Transportation," zoning district to Planned Development District No. 104 ("PD-104").

Section 3. That the development program statement, attached hereto as Attachment C and made a part hereof, and the concept plan, attached hereto as Attachment D and made a part hereof, are hereby approved. The approved development program statement and concept plan, which include the incorporation of the provisions of Cincinnati Municipal Code Chapter 1429, "Planned Development Districts," in addition to other more specific provisions, shall govern the use and development of the Property during the effective period of PD-104.

Section 4. That, should PD-104 lapse pursuant to provisions of Cincinnati Municipal Code Chapter 1429, "Planned Development Districts," the Property shall revert to the OG-T, "Office General-Transportation," zoning district in effect immediately prior to the effective date of PD-104.

Section 5. That this ordinance shall take effect and be in force from and after the earliest period allowed by law.

Passed:		_, 2025	
			Aftab Pureval, Mayor
			Altao Fuleval, Mayol
Attest:	Clerk	_	

Honorable City Planning Commission Cincinnati, Ohio

<u>SUBJECT:</u> A report and recommendation on a proposed zone change from Office General-Transportation (OG-T) to Planned Development (PD), including a Concept Plan and Development Program Statement, at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn.

GENERAL INFORMATION:

Location: 198 E. McMillan Street and 237 William H. Taft Road, Cincinnati, OH, 45219

Petitioner/Owner: Union on Taft LLC, 2718 Short Vine Street, Cincinnati OH, 45219

Request: A proposed zone change from Office General-Transportation Corridor (OG-T) to Planned Development (PD), including the Concept Plan and Development Program Statement, to allow for a mixed-use development to include an existing multi-family apartment building and a new restaurant withdrive-through service.

ATTACHMENTS:

Provided in addition to this report are the following exhibits:

- Exhibit A Location Map
- Exhibit B Concept Plan and Site Plan
- Exhibit C Development Program Statement
- Exhibit D Architecture Drawings
- Exhibit E Signage Plan
- Exhibit F Development Schedule
- Exhibit G Land Survey
- Exhibit H Deed
- Exhibit I Coordinated Site Review
- Exhibit J Traffic Analysis Report
- Exhibit K DOTE Letter
- Exhibit L Applicant Letter
- Exhibit M Community Engagement Summary
- Exhibit N Public Comment
- Exhibit O Site Proximity to Student Housing
- Exhibit P Legal Description
- Exhibit Q Zoning Plat

BACKGROUND:

The subject property is an approximately 5.5-acre site at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn. It is generally bound by William H. Taft Road to the north, Highland Avenue to the east, and E. McMillan Road to the south. The site has an existing four-story apartment building (The Union on Taft apartments) ranging from studios to five-bedroom units with surrounding surface parking and open space. The site was formerly used as an office building before the applicant converted the offices into 103 apartment units for student housing in 2023. The property is within the Mt. Auburn neighborhood and adjacent to the Corryville neighborhood.

The proposed Planned Development (PD) includes a new use for a drive-through restaurant operated by Chick-fil-A Inc. The PD is necessary for the applicant because a restaurant with a drive-through service is not permitted in the existing zoning district (OG-T). The Department of City Planning and Engagement

(DCPE) reviewed the proposed restaurant during the Coordinated Site Review process. A zone change to CC-M-T was recommended because it's the only adjacent zoning district that allows for drive-through service. The recommended zone change to CC-M-T would have required changes to the proposed site plan to better comply with the zone's design standards, such as placing the building at the corner along the sidewalk, adding 50-60% transparency to the east and south elevations, and relocating the drive-through to one of the side yards and/or rear yard. The applicant was unable to make the site changes for the proposed Chick-fil-A because the changes would disrupt the restaurant's optimal traffic flow, as discussed in Exhibit L. A PD is the remaining option for the applicant to receive zoning approval for the proposed restaurant.

The request is being made by Union on Taft LLC, an Uptown Rental Properties controlled entity. Uptown Rental Properties is a commercial and residential development and management company based in Cincinnati, OH. Their portfolio includes 37 commercial properties, 44 conventional residential properties, and 88 student properties.

ADJACENT LAND USE AND ZONING:

The subject property is currently zoned Office General-Transportation Corridor (OG-T). The adjacent zoning and land uses are as follows:

North:

Zoning: Office General-Transportation Corridor (OG-T)

Existing Use: Hamilton County office buildings

East:

Zoning: Office General-Transportation Corridor (OG-T), Commercial Community-Mixed

Transportation Corridor (CC-M-T), and Commercial Community-Pedestrian-

Transportation Corridor (CC-P-T)

Existing Use: Multi-family housing, offices, and a fast-food restaurant with a drive-through

South

Zoning: Planned Development (PD) and Office General-Transportation Corridor (OG-T)

Existing Use: Multi-family apartment buildings and an office building

West:

Zoning: Office General-Transportation Corridor (OG-T)

Existing Use: Surface parking lot

PROPOSED DEVELOPMENT:

The proposed Concept Plan (Exhibit B) and Development Program Statement (Exhibit C) allow for a mixed use development on the 5.5-acre site. The existing apartment building (The Union on Taft) would remain on approximately 4 acres of the site and a new Chick-fil-A restaurant with drive-through would be developed on the remaining 1.47 acres of the site. The Chick-fil-A location is proposed on the existing surface parking lot in the southeast corner of the site at the corner of E. McMillan Street and Highland Avenue. Chick-fil-A locations like the proposed restaurant, on average, complete 150-200 transactions per hour and create 80-120 jobs.

The applicant intends to later subdivide the land for the two uses. Union on Taft LLC would maintain ownership of both properties while managing the apartment building and leasing the land to Chick-fil-A.

Future Uses

The applicant has outlined the following uses to be permitted in the proposed Planned Development:

Component	Uses	Building Square Footage	Height	Parking
Phase one: The Union on Taft	Residential multi- family (103 units)	197,000 square feet	4 stories Up to 70 feet	180 spaces
Phase two: Chick-fil-A	Restaurant, limited, with Drive-Through	5,020 square feet	1 story Up to 21 feet	76 spaces

Buildings

Two buildings would exist in the proposed Planned Development. The first is the existing Union on Taft apartment building, which is 70 feet tall and 197,000 square feet. The building is located on the north and west half of the site. No changes are proposed to this building at this time.

The second building is the proposed Chick-fil-A. This would be a one-story building, approximately 21 feet in height and 5,020 square feet. The building would be located in the southeast corner of the site and setback approximately 52 feet on E. McMillan Street and 45 feet on Highland Avenue. The main entrance would be located facing west on the site, with staff entrances facing east towards Highland Avenue. The restaurant would provide seating for up to 90 guests inside and four guests outdoors.

Two canopy structures are proposed on the Chick-fil-A site (Exhibit D). The first canopy would be located south of the primary building along E. McMillan Street. The canopy would be approximately 60 feet long. The second canopy would be located along the north façade and would be approximately 31 feet by 50 feet.

The restaurant's refuse and storage area would be located in the northwest corner of the Phase Two site and enclosed with the same materials used on the primary building (Exhibit D).

Parking

The existing site includes 325 surface parking spaces. The proposal would maintain 180 parking spaces for the Union on Taft Apartments and 76 parking spaces for the Chick-fil-A restaurant. Driveway entrances for the Union of Taft Apartments would exist on William H Taft Road and E. McMillan Street, while Chick-fil-A would have separate driveway entrances on E. McMillan Street and Highland Avenue. The existing E. McMillan Street driveway, closest to the corner of Highland Avenue, would be moved approximately 100 feet west from its current location.

Signage

The applicant has submitted a proposed signage plan for Phase Two, including five sign typologies (Exhibit E):

- 1. Main ID Sign (A1): A ground sign located along E. McMillan Street, measuring 9 feet by 8 feet (72 square feet) on a 2-foot masonry base. The upper half of the sign would display the business logo, and the bottom half would provide a non-digitized message board with 6-inch letters.
- 2. Shared Monument Sign (A2): A ground sign located near the corner of William H Taft Road and Highland Avenue, measuring 9 feet 4 inches by 7 feet 4 inches (63 square feet) on a 2-foot masonry base. The upper half of the sign would display the restaurant's business logo, and the bottom half would display the apartment building's logo.

- 3. Wall Sign-Script (B, C, E): A wall sign on the west, south, and east façades, measuring 11 feet 9 inches by 5 feet (58.75 square feet each). The sign would display a secondary business logo.
- 4. Wall Sign-Icon (D): A wall sign on the north façade, measuring 6 feet by 6 feet (36 square feet). The sign would display a secondary business logo.
- 5. Welcome Sign (F): A wall sign on the west façade, measuring 5 feet 6 inches by 4 feet 8.75 inches (26 square feet). The sign would display the message, "Welcome Friends and neighbors ... McMillan & Highland".

Staff from the Department of City Planning and Engagement (DCPE) supports the proposed signage plan with the exception of the *Shared Monument Sign (A2)*. If the proposed Planned Development were approved and the phases subdivided, this proposed ground sign at William H Taft Road and Highland Avenue would create a non-conforming off-site outdoor sign. DCPE suggests that the *Shared Monument Sign (A2)* be removed from the signage plan if the Concept Plan and Development Statement are approved.

Circulation and Access

The circulation of the two phases would operate independently. The Union on Taft Apartment building and the Chick-fil-A sites would be separated and buffered by landscaping, fences, and retaining walls.

The Union on Taft Apartments has vehicular access from driveway entrances on William H Taft Road and E. McMillan Street. An internal driveway provides through access and connects the entrances. Sidewalks border the apartment site on its north, east, and south sides, and pedestrian walkways abut the building on most sides. No walkways or crosswalks are proposed to connect the apartment's pedestrian network to the proposed Chick-fil-A on site.

The proposed Chick-fil-A would have vehicular access from a separate driveway entrance on E. McMillan Street and an entrance on Highland Avenue. A two-way drive would funnel traffic from both entrances towards a two-lane drive-through circling the structure on the north, east, and south facades (Exhibit B). Sidewalks would border the restaurant along most of its sides. A pedestrian access point is proposed off each E. McMillan Street and Highland Avenue. The Highland Avenue pedestrian access provides a sidewalk along the northern edge of the drive-through and a crosswalk at the end of the drive-through lanes. The E. McMillan Street pedestrian access provides a northbound sidewalk near the start of the drive-through lanes with two crosswalks to the building's main entrance.

Common Open Space

The Union on Taft Apartments includes an outdoor courtyard for residents along Highland Avenue with lounge areas and grilling stations. The Chick-fil-A would provide an outdoor area with dining for up to four guests along the west façade. The Union on Taft LLC would secure maintenance for the common open space areas.

Fences and Walls

The border between the two phases is defined by the existing four-foot chain link fence atop concrete retaining walls, which are proposed to remain, except for a 20-foot reduction to one internal wall running east-west. Phase Two proposes a new two-foot block retaining wall with a 3.5-foot fence along most of the site's Highland Avenue edge, continuing around the corner onto E. McMillan Street for approximately 35 feet.

Landscaping

Final landscaping will be submitted with the Final Development Plan. The Concept Plan proposes that the impervious ratio of the site would be 0.73, where approximately 4.02 acres of the total 5.51 acres would be impervious surfaces.

Schedule

The applicant anticipates the development occurring under the following phasing program (Exhibit F):

Phase One: Renovation of the Union on Taft Apartment building was completed in 2023 and opened in 2024. This phase is considered complete, and no changes are proposed.

Phase Two: Construction of the Chick-fil-A restaurant would begin in September 2025 with an anticipated opening in February 2026.

SPECIFIC PURPOSES OF A PLANNED DEVELOPMENT DISTRICT:

Per § 1429-01 of the Cincinnati Zoning Code, *Specific Purposes*, the specific purposes of the PD Planned Development District are to:

- a. Establish a procedure for the development of land in order to allow for a more efficient and economic development of property than ordinarily permitted by conventional zoning and subdivision regulations.
 - The proposed development is not more efficient and economical than permitted by conventional zoning. Constructing a one-story restaurant without drive-through service is permitted with conditions in the existing OG-T zoning district. Maintaining OG-T zoning would allow for higher potential use of the site and future development to be more efficient and economical. For instance, a mixed-use building up to 112 feet tall can be built with eating and drinking establishments on the ground floor.
- b. Ensure orderly and thorough planning and review procedures that lead to quality design and development.
 - The planning and review procedures did not lead to quality design and development. Staff from the Department of City Planning and Engagement recommended the applicant team consider a re-design of the restaurant site to better comply with a zone change to the adjacent CC-M-T zoning district. The redesigned site would have brought the building to the sidewalk for pedestrians to engage with, and safely access, the restaurant while de-emphasizing the drive-through's visual impact.
- c. Encourage creativity in developments by allowing greater flexibility in access, light, open space and amenities.
 - The proposed development does not encourage creativity in access, light, open space, and amenities.
- d. Encourage common open space and provide for its maintenance.
 - The proposed development provides common open space and the applicant will provide for its maintenance.
- e. Encourage the coordinated development of properties that might otherwise be developed individually, which may be a detriment to the surrounding neighborhoods and the developer.
 - The proposed development does encourage the coordinated development of properties that might be developed individually, as Uptown Rental Properties will maintain ownership of both phases.

BASIC REQUIREMENTS OF A PLANNED DEVELOPMENT DISTRICT:

Per §1429-05 of the Cincinnati Zoning Code, *Basic Requirements*, PD Districts and development within PD Districts must comply with the following:

a. **Minimum Area** – The minimum area of a PD must be two contiguous acres.

The proposed zone change area is approximately 5.5 contiguous acres (Exhibit G).

b. **Ownership** – Evidence that the applicant has sufficient control over the tract of land to affect the proposed plan, including a list of all ownership and beneficial interests in the tract of land and the proposed development are required.

The applicant has provided proof of ownership of the property (Exhibit H).

c. **Multiple Buildings on a Lot** – More than one building is permitted on a lot.

The proposed Concept Plan (Exhibit B) and Development Program Statement (Exhibit C) indicate two buildings on the site, including the existing Union on Taft apartment building and proposed Chick-fil-A restaurant.

d. **Historic Landmarks and Districts** – Whenever a Planned Development application is filed for a property wholly or partially located within a historic landmark, historic district, or involving a historic structure, the Historic Conversation Board shall advise the City Planning Commission relating to approval of the Final Development Plan.

No portion of the site is located within a historic district, nor does it contain any historic landmark.

e. **Hillside Overlay Districts** – Whenever a Planned Development application is filed for a property wholly or partially located within a Hillside Overlay District, the City Planning Commission shall approve the Final Development Plan.

No portion of the site is located within the Hillside Overlay District.

f. Urban Design Overlay District – Whenever a Planned Development application is filed for a property wholly or partially located within an Urban Design Overlay District, the City Planning Commission shall approve the Final Development Plan.

No portion of the site is located within an Urban Design Overlay District.

CONCEPT PLAN AND DEVELOPMENT PROGRAM STATEMENT:

According to §1429-09 of the Cincinnati Zoning Code, *Concept Plan and Development Program Statement*, a petition to rezone a property to a Planned Development (PD) must include a Concept Plan and Development Program Statement. The purpose is to describe the proposed use or uses to be conducted in the PD District. The Concept Plan and Development Program Statement must include text or diagrams that specify:

a. **Plan Elements** – A survey of the tract to be developed, providing a metes and bounds description of the property and the survey of property lines and total acreage. Additionally, the plan should include the location in general terms, of land areas to be developed, including: type and description of proposed land uses, buildings and structures; street rights-of-way and driveways; parcel boundaries and proposed lots, including set back lines; building heights; pedestrian circulation systems and open space or other facilities; and proposed topography, drainage, landscaping and buffer plantings.

The petitioner has submitted a Concept Plan (Exhibit B) and Development Program Statement (Exhibit C) that includes sufficient information regarding proposed uses, building locations, street access, and open space and landscaping.

b. **Ownership** – Evidence that the applicant has sufficient control over the tract of land to affect the proposed plan, including a list of all ownership and beneficial interests in the tract of land and the proposed development.

The applicant has provided proof of ownership of the property (Exhibit H).

c. **Schedule** – Time schedule of projected development, if the total site is to be developed in phases or if construction is to extend beyond a two-year time period.

There are two anticipated phases for the overall development (Exhibit F). Phase One includes the completed development of the Union on Taft Apartments and has no proposed changes, while Phase Two will include a proposed restaurant with drive-through service. The restaurant construction is expected to be completed within one year.

d. **Preliminary Reviews** – A preliminary review of geo-technical, sewage, water, drainage and refuse collection.

The proposed development was preliminarily reviewed by City departments and reviewing agencies through the City's Coordinated Site Review process (Exhibit H), including the City's Metropolitan Sewer District (MSD), Stormwater Management Utility (SMU), and Greater Cincinnati Water Works (GCWW).

e. **Density and Open Space** – Calculations of density and open space area.

Approximately 73% of the site will be occupied by buildings or paved surfaces, leaving 27% of the site as open space.

f. **Other Information** – Any other information requested by the Director of City Planning or the City Planning Commission.

Signage

The applicant has submitted a proposed signage plan for Phase Two, including five sign typologies (Exhibit E):

- 1. Main ID Sign (A1): A ground sign located along E. McMillan Street, measuring 9 feet by 8 feet (72 square feet) on a 2-foot masonry base. The upper half of the sign would display the business logo, and the bottom half would provide a non-digitized message board with 6-inch letters.
- 2. Shared Monument Sign (A2): A ground sign located near the corner of William H Taft Road and Highland Avenue, measuring 9 feet 4 inches by 7 feet 4 inches (63 square feet) on a 2-foot masonry base. The upper half of the sign would display the restaurant's business logo, and the bottom half would display the apartment building's logo.
- 3. Wall Sign-Script (B, C, E): A wall sign on the west, south, and east façades, measuring 11 feet 9 inches by 5 feet (58.75 square feet each). The sign would display a secondary business logo.
- 4. Wall Sign-Icon (D): A wall sign on the north façade, measuring 6 feet by 6 feet (36 square feet). The sign would display a secondary business logo.
- 5. Welcome Sign (F): A wall sign on the west façade, measuring 5 feet 6 inches by 4 feet 8.75 inches (26 square feet). The sign would display the message, "Welcome Friends and neighbors ... McMillan & Highland".

FINAL DEVELOPMENT PLAN:

Under §1429-13 of the Cincinnati Zoning Code, a Final Development Plan shall be submitted to the City Planning Commission for any portion of an approved Concept Plan that the petitioner wishes to develop following approval of the Concept Plan, Development Program Statement, and Planned Development designation by City Council. The Final Development Plan must substantially conform to the approved Concept Plan and Development Program Statement and requires significantly more detail than the Concept Plan. Approval of the Final Development Plan will allow the applicant to obtain the necessary permits for development. The process allows the City Planning Commission to authorize staff to approve Minor Amendments that might be necessary and establishes the process for Major Amendments that must be reviewed and approved.

COORDINATED SITE REVIEW:

Phase Two (drive-through restaurant, Chick-fil-A) was reviewed by City departments and reviewing agencies through the City's Coordinated Site Review (CSR) process in September 2024 (Exhibit I). The Department of City Planning and Engagement recommended a zone change to Commercial Community-Mixed-Transportation Corridor (CC-M-T) with the need for additional conditions and variance relief, including drive-through and building location, setbacks, parking location, and building transparency.

A meeting with the applicant was held on October 1, 2024, to allow the development team the opportunity to discuss the comments outlined by each of the departments. Staff from the Department of City Planning and Engagement communicated that the department would have difficulty supporting the variances required and recommended that the applicant update the site plan to better conform with the proposed CC-M-T zoning district. Recommendations included placing the building at the corner along the sidewalk, adding 50-60% transparency to the east and south elevations, and relocating the drive-through to one of the side yards. The recommended changes are not out of character for the restaurant chain, as a similarly designed Chick-fil-A restaurant exists in the region.

The applicant team worked with the Department of Transportation and Engineering (DOTE) on the requirements for traffic analysis, including trip generation and distribution. On February 21, 2025, DOTE confirmed their completed review of the Traffic Analysis Report (Exhibit J), submitted by SHA Engineering, LLC, for the proposed Chick-fil-A and approved the findings and recommendations of the report (Exhibit K). Based on the report, DOTE determined that no changes were necessary for the right-of-way to support the traffic of the proposed restaurant. The report's recommendations included:

- The Highland Avenue driveway shall be constructed with a single inbound and single outbound lane.
- A stop sign shall be installed on the eastbound approach on the Highland Avenue driveway.
- The E. McMillan Street driveway shall be constructed with a single inbound and single outbound lane.
- A stop sign shall be installed on the southbound approach on the E. McMillan Street driveway.

ADDITIONAL CORRESPONDENCE:

On January 7, 2025, staff from the Department of City Planning and Engagement (DCPE) met with the applicant team to discuss the proposed site design and zone change progress. The applicant team shared in an email prior to the meeting that they were unable to make the changes to the site that the staff suggested due to the following reasons:

- Relocating the drive-through to the Highland Avenue side yard would have removed the driveway entrance along Highland Avenue, which the applicant team stated was "essential for the traffic flow of the site and is necessary for access to the apartment building loading dock/trash compactor" (Exhibit K).
- The drive-through in the E. McMillan Street side yard "did not work because of the west property line as well as the large retaining wall that goes through the site."

• Chick-fil-A is "closing locations that they opened just a few years ago because of design flow on the site/site size cannot accommodate the amount of traffic."

The applicant team made the following revisions, which have been incorporated into the current Concept Plan and Development Program Statement:

- "Increased the landscape buffer. On E. McMillan Street, there is a 5 foot sidewalk, 5 foot tree lawn (per DOTE requirements) in addition to a planted landscape buffer. On Highland Avenue, there is a 7 foot sidewalk and a landscape buffer."
- "Privacy screening, masonry walls and fencing on E. McMillan Street. On E. McMillan Street where the order point is, they are proposing adding a privacy screening canopy built of brick with aluminum storefront infill panels. It is a substantial structure designed to bring the building to the street while screening cars."
- "Walls/fencing on Highland Avenue behind the landscape buffer there will be a masonry wall with a 3.5' fence on top."

Based on the site design, DCPE staff communicated to the applicant that the department could not support a zone change (with the necessary variances) to either of the adjacent commercial zoning districts (CC-P-T and CC-M-T), and suggested that a Planned Development would be the path forward for the drive-through restaurant, with the inclusion of the existing multi-family apartment building.

On March 5, 2025, DCPE staff met with the applicant team and shared concerns about the site's lack of pedestrian connectivity. Following the meeting, the applicant team resubmitted their site plan to add pedestrian walkways from both Highland Avenue and E. McMillan Street with internal crosswalks to the building's main entrance (Exhibit B).

PUBLIC COMMENT AND NOTIFICATION:

The applicant team has engaged with the Mt. Auburn Community Council (MACC), as reported in Exhibit M. The applicant attended three MACC meetings in August 2024, October 2024, and February 2025. In addition, they have met with Mt. Auburn and Corryville Community Development Corporations, the University of Cincinnati, and the Port of Greater Cincinnati. Staff from the DCPE attended the February 18, 2025, MACC meeting regarding the proposed Chick-fil-A, where the applicant presented the project and engaged with the neighborhood regarding traffic concerns. Staff from the DOTE also attended the meeting and shared the department's acceptance of the applicant's Traffic Analysis Report and stated no changes are necessary to the right-of-way to support the traffic for the proposed restaurant. Some MACC members requested that the report be shared with the community council and that DOTE return to discuss the trip generation figures in more detail. MACC has not held a vote to determine support for the proposal, so a letter of support has not been submitted.

The Department of City Planning and Engagement held a virtual Public Staff Conference on the proposed Concept Plan and Development Program Statement, and concurrently submitted Final Development Plan, on March 6, 2025. Three members of the public were in attendance. One MACC member provided public comment, which included praise to the applicant team for their continued collaboration with MACC on the proposal and their excitement that the E. McMillan Street driveway is moving further west away from the Highland Avenue intersection. The members' concerns were related to the potential for traffic congestion resulting from vehicles turning left off Highland Avenue to access the restaurant and the number of drive-through stacking spaces provided on site. The applicant team reiterated DOTE's approval of the Traffic Analysis and stated that if changes were needed on Highland Avenue, then DOTE would require interventions.

Notice of the March 6, 2025, Public Staff Conference and April 4, 2025, City Planning Commission

meeting was sent to all property owners within 400 feet of the site, Mt. Auburn Community Council, and Corryville Community Council. One member of the public submitted a letter in opposition of the proposed design, as reported in Exhibit N.

CONSISTENCY WITH PLANS:

Plan Cincinnati (2012)

The proposal is consistent with the following *Plan Cincinnati* sections.

- Compete Initiative Area of *Plan Cincinnati*, specifically the goal to "Target investment to geographic areas where there is already economic activity" and the objective to "Focus development on the existing centers of activity" (p. 115-116).
- Connect Initiative Area, specifically the goal to "Expand options for non-automotive travel," the objective to "Expand connectivity and facilities for pedestrians, bicyclists, and transit users," and strategies "For all new development proposals, encourage pedestrian connections through sidewalks and other pedestrian facilities between private property and the public right-of-way" and "Review all new residential, commercial and mixed use developments to incorporate or extend multi-modal transportation networks (roads, bicycle paths, pedestrian systems)" (p.130-131).

The proposal adds a restaurant near the University of Cincinnati and student developments in Mt. Auburn and Corryville, aligning with the plan's goals of investing in and developing sites close to other economic activity. In addition, the applicant listened to feedback and updated the proposed restaurant site plan to include walkways connecting to the sidewalks along Highland Avenue and E. McMillan Street with internal crosswalks to the building's main entrance. These updates expand the options for non-auto-motive travel and encourage pedestrian connections between private property and public right-of-way.

The proposal is not consistent with the following *Plan Cincinnati* sections.

- *Policy Principle* to "Preserve or create a pedestrian-scaled city" (p. 78). The policy's language states, "we want Cincinnati to become truly human-scaled by focusing on accommodating the person and not the automobile," and continues in the policy recommendation that, "We will permeate our neighborhoods with compact, walkable mixed-use development, bikeable streets and trails, and transit of all types."
- *Live* Initiative Area, specifically the objective to "Increase mixed-use, compact walkable development throughout the basin and uptown, surrounding our centers of activity, and along transit corridors" (p. 157).
- Sustain Initiative Area, specifically the goal to "Make sustainable access to and use of fresh, healthy food a priority in all neighborhoods," the objective to "Eliminate Food Deserts in Cincinnati," and the strategy to "Consider a program to regulate the number and nature of fast-food establishments in the City" (p. 190-191).

The proposed drive-through restaurant is an auto-oriented use and fast-food establishment not supported by *Plan Cincinnati*. The plan strongly encourages development in the City that is designed for pedestrians and increases walkable, mixed-use developments. The proposal fails to meet the *Policy Principle* and *Live* strategies based on its design, which is primarily focused on attracting and circulating vehicles. The restaurant's heavy emphasis on its drive-through, including two lanes circling the building, forces the restaurant to be set back more than 45 feet from the street edge. This creates an uncomfortable buffer for pedestrians and the public space. In addition, the Planned Development is not an integrated mixed-use development as it does not connect the site's two uses or function as a mixed-use environment. Finally,

the fast-food nature of the proposed restaurant opposes the *Sustain* strategy to regulate fast-food establishments in the City and prioritize healthier food options.

Green Cincinnati (2023)

The proposal is consistent with the *Mobility* Focus Area of *Green Cincinnati*, specifically, the strategy to "Improve bike and pedestrian connectivity so that residents can safely access the places they need to go" and the action to "Improve sidewalk connectivity and maintenance in neighborhoods where connectivity/walkability is lacking" (p. 118-119). The applicant listened to feedback and updated the proposed restaurant site plan to include walkways connecting to the sidewalks along Highland Avenue and E. McMillan Street with internal crosswalks to the building's main entrance.

Mt. Auburn Community Plan (1992)

The proposal is consistent with the Commercial Development section of the *Mt. Auburn Community Plan*, specifically the goal to "Limit retail business development to centralized locations near transit and residences" (p. 46). The proposed restaurant is centrally located near several multi-family apartment buildings and student housing along multiple transit routes.

CITY PLANNING COMMISSION ACTION:

According to §1429-11(a) of the Cincinnati Zoning Code, the City Planning Commission may recommend approval or conditional approval, with restrictions on the establishment of a PD District on finding that all of the following circumstances apply:

- 1. The PD Concept Plan and Development Program Statement are consistent with applicable plans and policies and is compatible with surrounding development;
 - The proposed Concept Plan and Development Program Statement do not fully align with *Plan Cincinnati* (see *Consistency with Plans*) but are compatible with several sections of the *Green Cincinnati* plan, *Mt. Auburn Neighborhood Plan*, and surrounding development.
- 2. The PD Concept Plan and Development Program Statement enhance the potential for superior urban design in comparison with the development under the base district regulations that would apply if the plan were not approved;

The Concept Plan and Development Program Statement fails to enhance the potential for superior urban design compared to base zoning regulations. To meet the minimum design standards under the property's current zoning regulations, the applicant would need to:

- Request a conditional use for the building size of the restaurant (§1407-05)
- Remove the drive-through (§1407-05)
- Locate parking in the rear yard (§ 1407-04)
- Remove the ground sign near the corner of William H. Taft Road and E. McMillan Street (§ 895-1-O)
- Subdivide the lot into two properties for each use (the applicant intends to subdivide uses if the PD is approved)

Staff from the Department of City Planning and Engagement (DCPE) recommended that the applicant team revise the restaurant site plan to better comply with a zone change to the adjacent CC-M-T zoning district. Recommendations included placing the building at the corner along the sidewalk, adding 50-60% transparency to the fronting facades, and relocating the drive-through to one of the side yards. The redesigned site would have brought the building to the sidewalk for pedestrians to engage with, and safely access, the restaurant while de-emphasizing the drive-through's visual impact. The applicant provided reasons why the recommended site designs were not feasible (see *Additional*

Correspondence).

DCPE believes the proposed site design does not provide a superior urban design, and the proposed restaurant use could exist within the City's zoning code in several forms.

3. Deviations from the base district regulations applicable to the property at the time of the PD application are justified by compensating benefits of the PD Concept Plan and Development Program Statement;

The proposed Planned Development can provide compensating benefits that justify deviations from the base district regulations. The deviations from the base district regulations that the proposed Planned Development is requesting are the building size, drive-through service, drive-through location, parking location, and a potential off-site outdoor advertising sign. The site for the proposed restaurant is currently a surface parking lot. Developing this site and replacing the parking lot with a restaurant can activate a corner lot on an arterial street, provide over 80-120 jobs, increase the land's value, improve the site's infrastructure, and increase the City's tax revenue.

4. The PD Concept Plan and Development Program Statement includes adequate provisions for utility services, refuse collection, open space, landscaping, pedestrian circulation and traffic circulation, building design and building location.

All aspects are outlined in the Concept Plan and Development Program Statement as submitted.

ANALYSIS:

The proposed Planned Development (PD) is intended to be subdivided to create two lots, one for each use if the Concept Plan, Development Program Statement, and Final Development Plan are approved. No improvements are proposed on the completed multi-family apartment site (Phase One), so approving the PD will only result in a drive-through restaurant (Phase Two). The staff from the Department of City Planning and Engagement (DCPE) acknowledges that the proposed PD can benefit the area by activating an existing surface parking lot and offering a new commercial use near an economic activity center. In addition, the proposed Chick-fil-A restaurant is compatible with the existing surrounding development and the more auto-centric nature of arterial thoroughfares near interstates. However, DCPE believes that the City must be careful about the placement of auto-oriented uses, especially in an expanding college environment that is driving increased density and student housing near the site (Exhibit O) and when City policies are focused on creating pedestrian-scaled, walkable, mixed-use developments.

Planned Developments are intended to allow for zoning flexibility in projects to create more efficient use of land than conventional zoning, align with City policies, and provide superior urban design. DCPE did not support the restaurant's auto-oriented site design when reviewing the restaurant for a zone change to one of the adjacent commercial districts (see *Coordinated Site Review* and *Additional Correspondence*). The restaurant could better accommodate pedestrians (including nearby residents, college students, and transit users), deemphasize vehicular impact, and substantially fit within the City's zoning code. In addition, maintaining OG-T zoning would allow for higher potential use of the site and future development to be more efficient and economical.

DCPE staff recognizes and appreciates the effort the applicant team has put into engaging the Mt. Auburn Community Council (MACC). The applicant attended several council meetings and hosted a site walk with their members. It is important for the City's developers to engage with the public. The main concerns from MACC and the public are related to the site's ability to capture vehicular demand for the restaurant, congestion on the adjacent roadways surrounding the site, and disruption to typical traffic in this area. DCPE defer to the Department of Transportation and Engineering review and its approval of the Traffic Analysis Report submitted by the applicant (Exhibit K).

FINDINGS:

It is the opinion of the Department of City Planning and Engagement staff that the proposed Concept Plan and Development Program Statement are in compliance with §1429-05 *Basic Requirements* but are not in compliance with §1429-11 (a) *City Planning Commission Action*. The proposal is not consistent with the standards of establishing a Planned Development District.

CONCLUSIONS:

Staff from the Department of City Planning and Engagement does not support the proposed zone change from Office General-Transportation Corridor (OG-T) to Planned Development (PD), including a Concept Plan and Development Program Statement, at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn for the following reasons:

- 1. The proposed PD is not consistent with the specific purposes of the Planned Development districts (§ 1429-01).
- 2. The proposed PD does not fully align with Plan Cincinnati (2012).
- 3. While the proposed PD would activate an existing surface parking lot, the site plan is auto-oriented and does not enhance the potential for superior urban design.
- 4. The proposed PD would only allow for the development of a drive-through restaurant, as no changes are proposed for the existing residential use. Further, the two uses would operate separately, not as a fully integrated mixed-use development.

RECOMMENDATION:

Staff from the Department of City Planning and Engagement recommends that the City Planning Commission take the following action:

- 1. ADOPT the Department of City Planning and Engagement Findings as detailed in this report; and
- 2. NOT APPROVE the Concept Plan and Development Program Statement as submitted; and
- 3. **DENY** the proposed zone change from Office General-Transportation (OG-T) to Planned Development (PD), including a Concept Plan and Development Program Statement, at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn.

Respectfully submitted:

Kyle Gibbs, City Planner

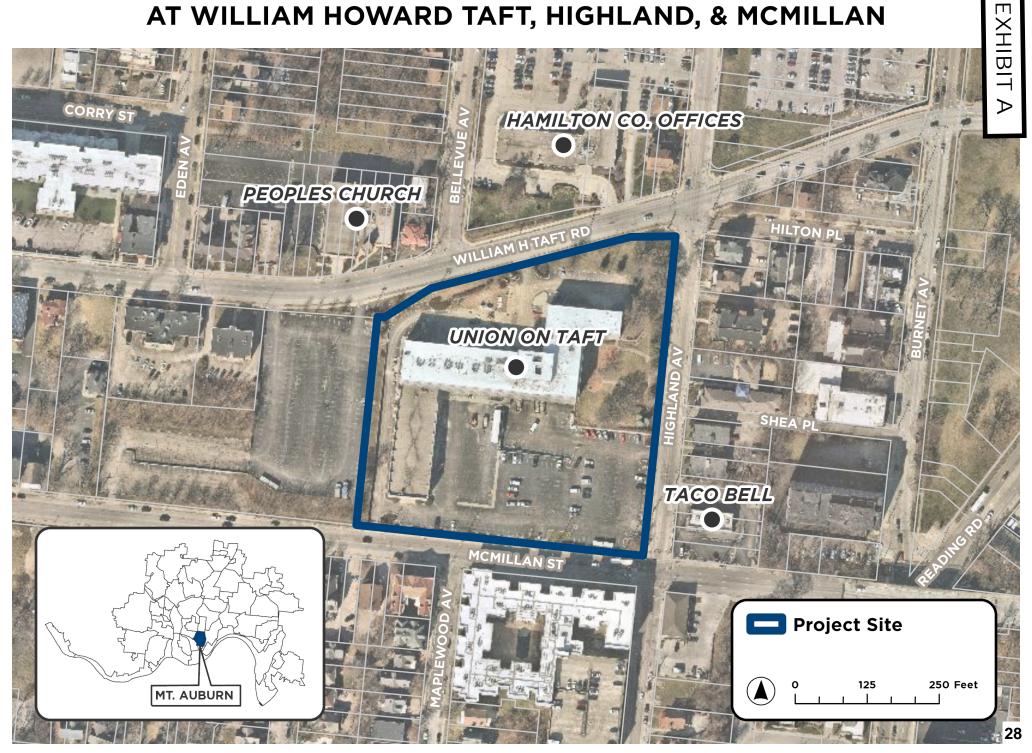
Department of City Planning & Engagement

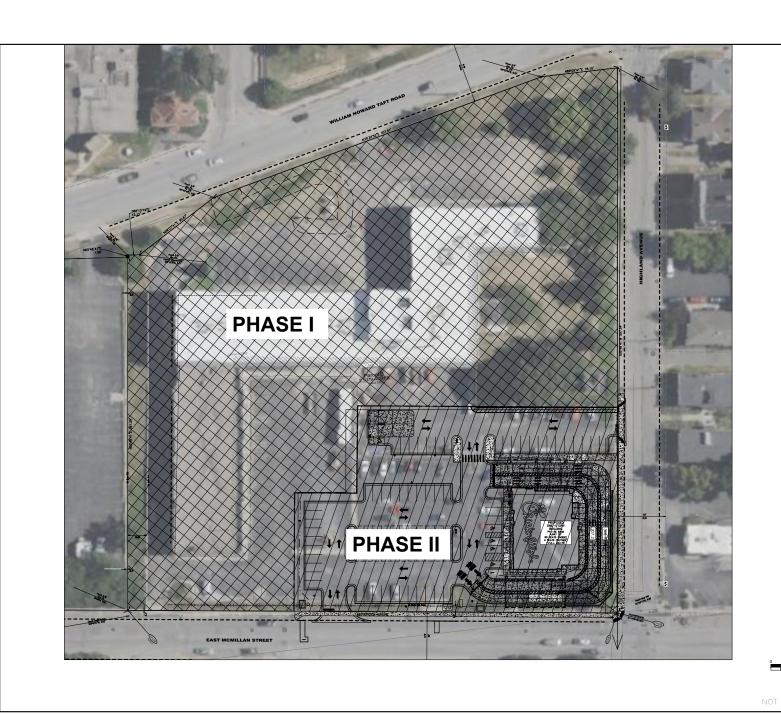
Approved:

Katherine Keough-Jurs, FAICP, Director Department of City Planning & Engagement

Katherie Keargh-Jus

A PROPOSED ZONE CHANGE TO PLANNED DEVELOPMENT (PD) AT WILLIAM HOWARD TAFT, HIGHLAND, & MCMILLAN







Chick-fil-A 5200 Buffington Road Atlanta, Georgia 30349-2998



CORRYVILLE (OH) FSU E MCMILLAN STREET & HIGHLAND AVE CINCINNATI, OHIO 45219

FSR# 05724

ZONING EXHIBIT

EXH

PRELIMINARY

UTILITY CONTACTS:

(513) 591-7858 RICK ROELL

DUKE ENERGY

CINCINNATI BELL (513) 668-5352

(513) 526-6726 ZACH HOFSTETTER

CITY OF CINCINNATI BUILDING AND INSPECTIONS (513) 352-3271 RODNEY RINGER

CITY OF CINCINNATI METROPOLITAN SEWER DISTRICT OF GREATER CINCINNATI (513) 244-5588 ROB KERN

CITY OF CINCINNATI STORMWATER MANAGEMENT UTILITY (513) 222-3643 KEVIN GOLD

CITY OF CINCINNATI GREATER CINCINNATI WATER WORKS

BUILDING:

ANITARY SEWER:

STORM SEWER

WATER SERVICE:

NATURAL GAS

TELEPHONE/CABLE/FIBER:



CORRYVILLE

E MCMILLAN STREET & HIGHLAND AVE CINCINNATI, OHIO 45219

ZONED

OFFICE GENERAL (OG)



LOCATION MAP

SCALE: 1" = 1000'

SHEET INDEX

SHEET NUMBER	SHEET TITLE
C-000	COVER
C-100	SITE DEMOLITION PLAN
C-200	SITE PLAN
C-300	GRADING PLAN
PS-100	PLUMBING SITE PLAN
PS-101	UTILITY TABLES
L-100	LANDSCAPE PLAN
L-101	NOTES AND DETAILS
L-102	LANDSCAPE
	SPECIFICATIONS

PAICK THEFE Chick-fil-A 5200 Buffington Road Atlanta, Georgia 30349-2998

 \Box



(OH) FSU Ŧ

E MCMILLAN STREET & HIGHLAND CINCINNATI, OHIO 45219 CORRYVILLE

FSR# 05724

PRELIMINARY

COVER

C-000

FLOOD CERTIFICATION:

PARCEL IS LOCATED WITHIN ZONE "X" (AREA OF MINIMAL PARCEL IS LOCATED WITHIN ZONE "X" (AREA OF M FLOOD HAZARD) AS INDICATED BY THE FLOOD INSURANCE RATE MAP (FIRM) MAP NUMBER 39061C0219E, EFFECTIVE DATE: 021772010 PUBLISHED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY

NOTES:

- WOOLPERT, INC. IS TO BE NOTIFIED 3 DAYS PRIOR TO CONSTRUCTION START.
- WOOLPERT, INC. SHALL BE INCLUDED IN ALL PRE-CONSTRUCTION MEETINGS.
- ANY DISCRIPANCIES ON THIS PLAN SET MUST BE NOTED AND WOOLPERT, INC. NOTIFIED PRIOR TO ACTUAL CONSTRUCTION SO THAT ANY DISCREPANCIES CAN BE RESOLVED.
- ANY WORK PERFORMED IN THE POW SHALL BE PERFORMED BY A CITY "QUALIFIED" CONTRACTOR AND MONITORED BY THE PUBLIC WORKS DEPARTMENT.
- AN AS-BUILT SURVEY AT THE END OF CONSTRUCTION IS REQUIRED. THE SURVEY SHOULD INCLUDE THE FOLLOWING INFORMATION. STATE PLAN COORDINATES (KLE) FOR ALL MANHOLES AND CATCH BASINS, INVERTS AND TOP ELEVATIONS FOR ALL MANHOLES AND CATCH BASINS, THE SLOPES, SIZES, AND MATERIALS FOR ALL STORM LINES.

DRAINAGE STATEMENT:

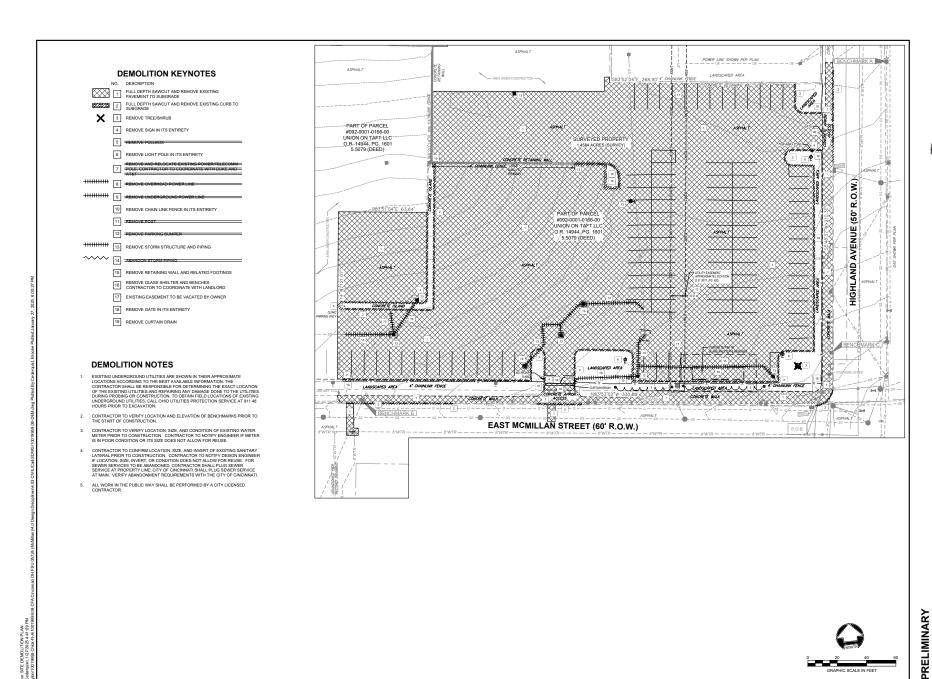
TO THE BEST OF MY KNOWLEDGE AND BELIEF, THE DRAINAGE OF SURFACE WATERS WILL NOT BE CHANGED BY THE PROPOSED DEVELOPMENT. IF ANY DRAINAGE PATTERNS WILL BE CHANGED, REASONABLE PROVISIONS HAVE BEEN MADE FOR THE REASONABLE PROVISIONS HAVE BEEN MADE FOR THE COLLECTION AND DIVERSION OF SUCH SUFFACE WATERS INTO THE PUBLIC AREA, OR DRAINS APPROVED FOR THE USE BY THE MUNICIPAL ENGINEER, AND THAT SUCH SUBFACE WATERS ARE PLANNED FOR IN ACCORDING WITH GENERALLY ACCEPTED ENGINEERING PRACTICES SO AS TO REDUCE THE LIKELIHOOD OF DAMAGES TO ADJOINING PROPERTIES.

SITE BENCHMARKS:

BENCH TIE SET IN POWER POLE #HMO36236E LOCATED AT THE INTERSECTION OF HIGHLAND AVENUE &

BENCH TIE SET IN POWER POLE #08 804, LOCATED SOUTHWEST OF THE SURVEYED PROPERTY, ON THE NORTH RIGHT-OF-WAY LING OF MCMILLAN STREET, NORTH OF THE INTERSECTION OF MAPLEWOOD AVENUE. ELEVATION = 825.39'

CITY OF CINCINNATI BENCHMARK NO. 327 SET AT THE NORTHWEST CORNER OF THE INTERSECTION OF MCMILLAN



EXHIBI⁻

Chick-fil-A 5200 Buffington Road Atlanta, Georgia 30349-2998



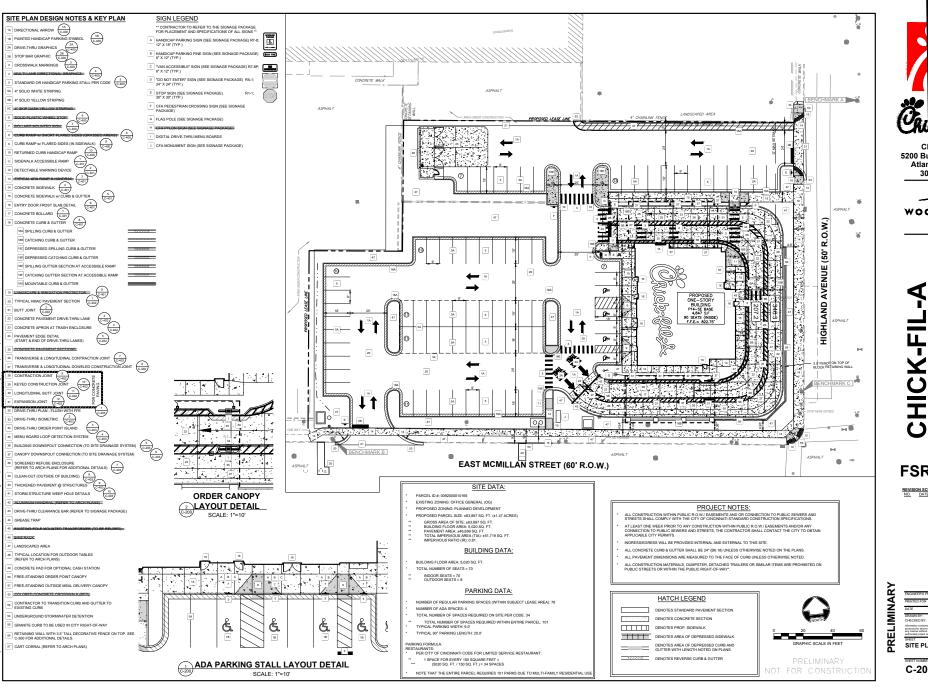
E MCMILLAN STREET & HIGHLAND AVE CINCINNATI, OHIO 45219 CORRYVILLE (OH) FSU CHICK

FSR# 05724

REVISION SCHEDULE
NO. DATE

SITE DEMOLITION PLAN

C-100





Chick-fil-A 5200 Buffington Road Atlanta, Georgia 30349-2998



CORRYVILLE (OH) FSU

FSR# 05724

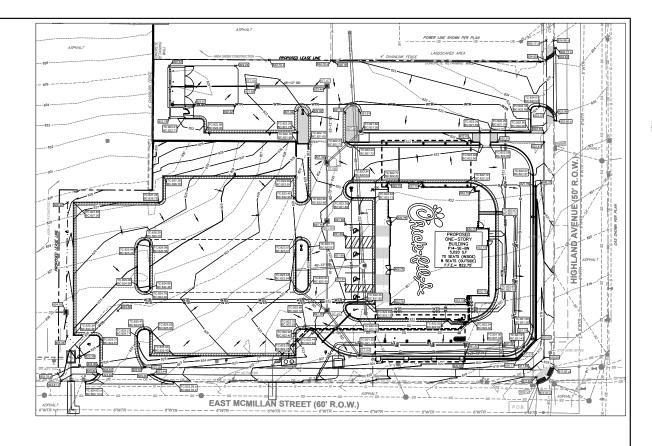
SITE PLAN

C-200

GRADING & DRAINAGE NOTES CONTRACTOR IS RESPONSIBLE FOR COURDING TON OF SHE FLOW DOCUMENTS AND ARCHITECTURAL DESIGN FOR EXACT BUILDING UTILITY CONNECTION LOCATIONS, GREASE TRAP REQUIREMENTS/DETAILS, DOOF

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- SITE GRADING SHALL BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND THE RECOMMENDATIONS SET FORTH IN THE GEOTECHNICAL REPORT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING AND REPLACING WITH SUITABLE MATERIALS AS SPECIFIED IN THE REMOVING AND REPLACING WITH SUTFABLE WATERIALS AS SECOPED IN THE GOTTENHOUS REPORT. ALL DEXAMPLES OF FILLED AREAS SHALL BE COMMUTED AN OUTLINED IN THE GOTTENHOUS REPORT MOSTING OF REPORT MOST THE COMMUNICATION OF THE CO REPRESENTATIVE, SUBBASE IS TO BE REMOVED AND FILLED WITH APPROVED FILL MATERIAL COMPACTED AS DIRECTED BY THE GEOTECHNICAL REPORT.
- INSTALLATION SHALL BE AS PERT THE RECOMMENDATIONS PROVIDED IN THE GEOTECHNICAL REPORT AND SHALL BE COORDINATED WITH THE APPLICABLE UTILITY COMPANY SPECIFICATIONS.
- THE CONTRACTOR SHALL COMPLY TO THE FULLEST EXTENT WITH THE LATES IGHA STIMMARS SHALE CEMENTARIS THE MY CONTRACT HAVING A STANDARD SHALE CEMENTARIS THE MY CONTRACT HAVING A STANDARD SHALE CEMENTARIS THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE "MEANS AND METHODS" REQUIRED TO MEET THE INTENT AND PERFORMANCE CRITERIA OF OSHA, AS WELL AS ANY OTHER ENTITY THAT THAS JURISDICTION FOR EXCAVATION AND/OT TRENCHING PROCEDURES.
- PAVEMENT SHALL BE SAW CUT IN STRAIGHT LINES TO THE FULL DEPTH OF THE EXISTING PAVEMENT. ALL DEBRIS FROM REMOVAL OPERATIONS SHALL REMOVED FROM THE SITE AT THE TIME OF EXCAVATION. STOCKPILING OF DEBRIS WILL NOT BE PERMITTED.
- THE TOPS OF EXISTING MANHOLES, INLET STRUCTURES, AND SANITARY CLEANOUT TOPS SHALL BE ADJUSTED, IF REQUIRED, TO MATCH PROPOSED GRADES IN ACCORDANCE WITH ALL APPLICABLE STANDARDS.
- THE CONTRACTOR IS RESPONSIBLE FOR WEREIGNATION OF EXISTING TOPOGRAPHIC INFO CONTRACTOR IS RESPONSIBLE FOR WEREIGNATION AND UTILITY WINGET ELEVATIONS PROOF TO TOPOGRAPHIC INFO CONTRACTOR IS AND UTILITY WINGET ELEVATIONS PROOF TO CONTRACT SUPPLIES AND CURSES. TO CONTRACT SUPPLIES AND CURSES TO CONTRACT SUPPLIES AND CURSES TO CONTRACT TO PROVENT OR PROJECT COST MUST BE EXISTING TO THE EXISTINGER AND WINGE TO CONTRACT SUPPLIES AND CURSES AND TO SUPPLIES AND CURSES AND THE MEDITAL TO PROVIDE AND THE MEDITAL TO PROVIDE AND THE MEDITAL TO PROJECT COST MUST BE EXISTINGED TO THE EXISTINGER AND WINGE TO CONTRACT AND THE CONTRACT AND THE
- PROPOSED TOP OF CURB ELEVATIONS ARE GENERALLY 6" ABOVE EXISTING LOCAL ASPHALT GRADE UNLESS OTHERWISE NOTED. FIELD ADJUST TO CREATE A MINIMUM OF 0.75% GUTTER GRADE ALONG CURB FACE. ENGINEER TO APPROVE FINAL CURBING CUT SHEETS PRIOR TO INSTALLATION.
- IN CASE OF DISCREPANCIES BETWEEN PLANS OR RELATIVE TO OTHER PLANS, THE SITE PLAN WILL TAKE PRECEDENCE. IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY CONFLICTS.
- CONTRACTOR SHALL SUPPLY A COPY OF APPROVALS TO ENGINEER AND OWNER PRIOR TO INITIATING WORK.
- SITE GRADING SHALL NOT PROCEED UNTIL EROSION CONTROL MEASURES HAVE BEEN INSTALLED.
- SEE EROSION CONTROL PLAN FOR EROSION CONTROL MEASURES AND NOTES.
- ALL EXISTING STRUCTURES, UNLESS OTHERWISE NOTED TO REMAIN, FENCING, TREES, & ETC., WITHIN CONSTRUCTION AREA SHALL BE REMOVED & DISPOSED OF OFF SITE. NO ON SITE BURNING WILL BE ALLOWED.
- ALL DRAINAGE STRUCTURES SHALL BE PRE-CAST.
- ALL DRAINAGE STRUCTURES AND STORM SEWER PIPES SHALL MEET HEAVY DUTY TRAFFIC (H20) LOADING AND BE INSTALLED ACCORDINGL
- GENERAL CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES HAVING UNDERGROUND UTILITIES ON SITE OR IN RIGHT-DF-AWAY PRIOR TO EXCAVATION. CONTRACTOR SHALL CONTRACT UTILITY LOCATING COMPANY AND LOCATE ALL UTILITIES PRIOR TO GRADING START.
- NO PART OF THE PROPOSED PROJECT IS LOCATED WITHIN A FLOOD HAZARD AREA
- SPOT ELEVATIONS SHOWN ARE @ EDGE OF PAVEMENT UNLESS OTHERWISE NOTED ON PLAN.
- ALL CONCRETE CURB & GUTTER SHALL BE TYPE B-6.18 CURB UNLESS OTHERWISE NOTED ON THE PLANS.
- ALL STORM SEWER JOINTS SHALL HAVE O-RING GASKETS.
- MATCH EXISTING GRADES AT PROPERTY LINES AND/OR CONSTRUCTION LIMITS.
- BACKELL TO THE TOP OF CURRS
- SITE SHALL BE GRADED TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS
- ALL SIDEWALK CROSS SLOPES SHALL BE A MAXIMUM OF 1.5%
- DESIGNATED HANDICAP PARKING AREAS SHALL BE GRADED TO A MAXIMUM OF 1.5%
- THE CONTRACTOR SHALL CONFINE HIS GRADING OPERATIONS TO WITHIN CONSTRUCTION I MITS AND EASEMENTS SHOWN ON THE PLANS, ANY DAMAGE TO PROPERTIES OUTSIDE THE SITE BOUNDARY SHALL BE AT THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL APPLY NECESSARY MOISTURE CONTROL TO THE CONSTRUCTION AREA AND HAUL ROADS TO PREVENT THE SPREAD OF DUST ALL FIELD TILES ENCOUNTERED SHALL BE REPLACED AND/OR CONNECTED TO THE STORM SEWER SYSTEM AND LOCATED AND IDENTIFIED ON THE RECORD PLANS BY THE CONTRACTOR.
- ALL STORM DRAINAGE CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE MOST CURRENT CITY OF CINCINNATI CONSTRUCTION AND MATERIAL SPECIFICATIONS.



GENERAL NOTES:

ACCESSIBLE PARKING, RAMPS, AND SIGNAGE SHALL COMPLY WIT FACILITIES. BUILTY GUIDELINES FOR BUILDINGS AND

1 WEEK PRIOR TO CONSTRUCTION WITHIN CITY OR STATE ROW OR ANY CONNECTION TO PUBLIC SEWERS, CONTRACTOR SHINOTIFY THE APPROPRIATE CITY ENGINEERING DIVISION.

CONTRACTOR TO VERIFY BUILDING DIMENSIONS WITH ARCHITECTURAL PLANS. PLACE 3/4 INCH EXPANSION JOINT BETWEEN ALL P.C.C. PAVEMENT 3 DIEWALKS AND BUILDING. PLACE 1/2 NCH EXPANSION JOINT BETWEEN SIDEWALKS AND P.C.C. PAVEMENT. CUTTURE XEXANSION JOINTS TO BE FLUSH WITH SUBFACE.

ALL PROPERTY PINS SHALL BF PROTECTED FROM GRADING OR OTHER OPERATIONS. ANY PINS DISTURBED SHALL BE RESET AT THE CONTRACTOR'S EXPENSE. DO NOT STORE CONSTRUCTION MATERIALS AND EQUIPMENT IN THE RIGHT-OF-WAY

THE CONTRACTOR SHALL NOT DISTURB DESIRABLE GRASS AREAS AND DESIRABLE TREES OUTSIDE THE CONSTRUCTION LIMITS. THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK OR SERVICE, VEHICLES AND EQUIPMENT OR USE THESE AREAS FOR STORAGE OR MATERIALS. STORAGE, PARKING AND SERVICE AREAS WILL BE SUBJECT TO THE APPROVAL OF THE OWNER THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY AREAS OF PAVEMENT OR SIDEWALK NOT TO BE REMOVED THAT IS DAMAGED DUE TO OPERATING EQUIPMENT ON THE PAVEMENT OR SIDEWALK.

THE CONTRACTOR MAY BE REQUIRED TO PLACE TEMPORARY WARNING DEVICES AND SAFETY FENCE AT CERTAIN LOCATIONS WHERE REPLACEMENT FEATURES ARE NOT INSTALLED THE SAME DAY, AS DIRECTED BY THE ENGINEER OR THE CITY. ALL CONSTRUCTION WITHIN PUBLIC ROWIEASEMENTS AND/OR ANY CONNECTION TO PUBLIC SEWERS AND STREETS, SHALL COMPLY WITH THE CITY CONSTRUCTION SPECIFICATIONS FOR SUBDIMISIONS AND LATEST EDITION OF QOOT DESIGN STANDARDS

CONTRACTOR TO GRADE 4" BELOW THE BACK OF CURB TO ALLOW FOR THE PLACEMENT OF TOPSOIL. A MINIMUM OF 4" OF TOPSOIL SHALL BE PLACED IN ALL PLANTING BEDS AND ALL GRASSED AREAS, GRADED AREAS TO BE HELD DOWN TO THE APPROPRIATE ELEVATION TO ACCOUNT FOR TOPSOIL. SEE SHEETL-101 FOR DETAIL-101 FOR DETAIL.

TRAFFIC CONTROL NOTES:

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIME TO EAST MCMILLAN STREET AND HIGHLAND AVENUE.

LATE TABLE CONTROL SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

SIDEWALK CLOSED SIGNS REQUIRED FOR ALL SIDEWALK CLOSURES.

THE CONTRACTOR IS CAUTIONED NEITHER TO OBSTRUCT NOR REMOVE ANY EXISTING PAVEMENT, NOR TO DISTURB THE EXISTING TRAFFIC PATTERNS MORE THAN IS NECESSARY FOR THE PROPER EXECUTION OF THE WORK.

HATCH LEGEND

DENOTES REVERSE (SPILLING) CURB & GUTTER DENOTES AREA OF DEPRESSED DENOTES CONCRETE CURB & GUTTER (CATCHING) DENOTES AREA OF DEPRESSED CATCHING CURB & GUTTER

ALL STORM STRUCTURES WITHIN PAVED AREAS REQUIRE WEEP HOLES. SEE DETAIL 40 & 40A ON SHEET C-403 FOR WEEP HOLE DETAILS. DENOTES PROPOSED OVERFLOW DIRECTION ARROW.



LIMINARY

Ш

GRADING PLAN

C-300

 \Box \Box ico-ku-L Chick-fil-A

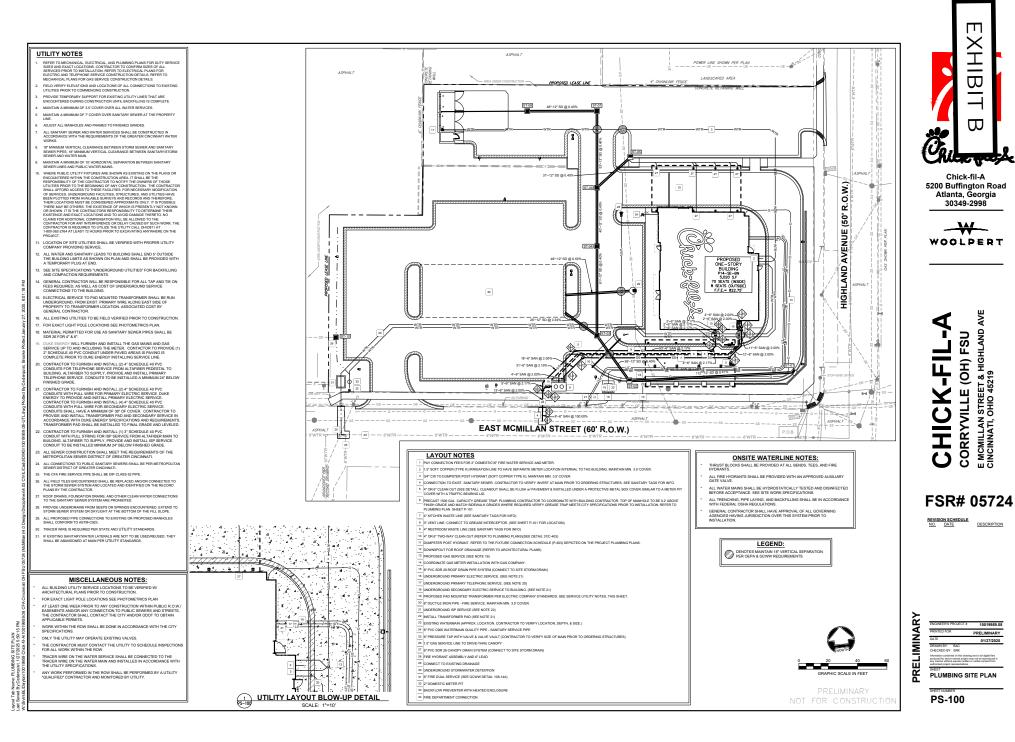
5200 Buffington Road Atlanta, Georgia 30349-2998

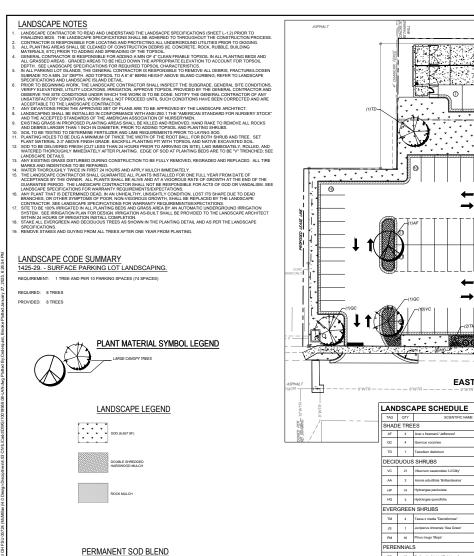


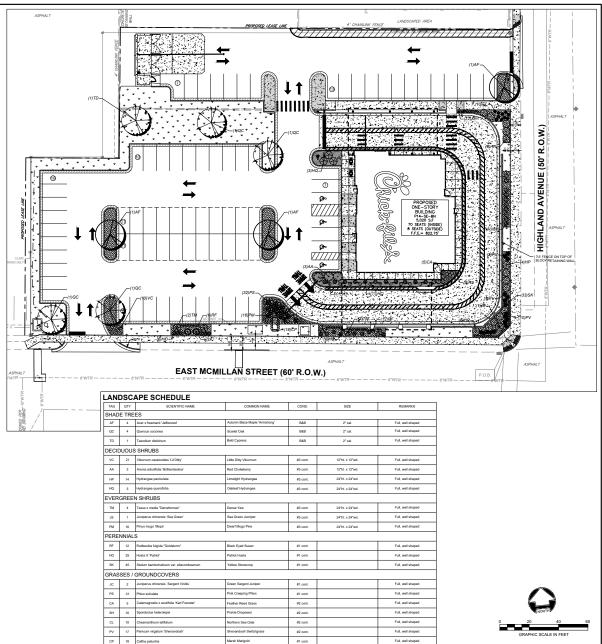
STREET & HIGHLAND OHIO 45219 FSU (HO) 4 CORRYVILLE MCMILLAN SINCINNATI, C I

FSR# 05724

REVISION SCHEDULE
NO. DATE







* QUANTITIES ARE PROVIDED FOR CONVENIENCE ONLY; CONTRACTOR IS RESPONSIBLE FOR ALL QUANTITIES OF PLANTS ON LANDSCAPE PLAN.

EXHIBIT B

Chick-fil-A 5200 Buffington Road Atlanta, Georgia 30349-2998



CHICK-FIL-ACORRYVILLE (OH) FSU EMCMILLAN STREET & HIGHLAND GINGINNATI, OHIO 45219

FSR# 05724

REVISION SCHEDULE
NO. DATE

DESCRIPTIO

ENGINEERS 1

PRINTED FOR
DATE

BRAWN BY:
CHECKED BY
Indicated in paint
of the painter without
authorized printed

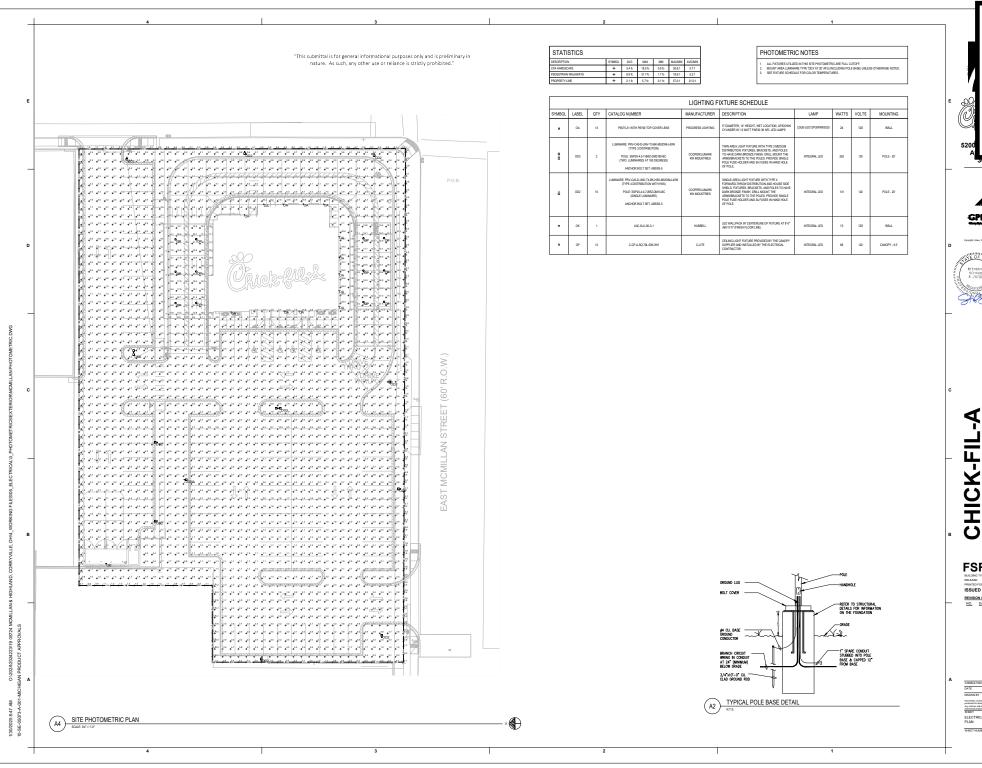
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DATE 01/2

DATE 01/2

DRAWN BY: CM
CHECKED BY: JM
Information contained on this drawing and in all dig
produced for above manual project may not be sign
authorizing repict in any not be sign
authorizing repict in greater table on the
ANDDSCAPE PLAN

LANDSCAPE PLAN

SHEET NUMBER
L-100









E MCMILLAN STREET & HIGHLAND AVE CINCINNATI, OHIO 45219 CHICK-FIL-A CORRYVILLE (OH) FSU

FSR#05724 ISSUED FOR PERMIT REVISION SCHEDULE

NO. DATE DESCRIPTION

E-102





Chick-fil-A – 198 E. McMillan Street Proposed PD District Program Statement (3-18-2025)

Union Taft LLC is the owner of approximately 5.5 acres at the northwest corner of E. McMillan Street and Highland Avenue, at 198 E McMillan Street. The property currently contains a parking lot on approximately 1.47 acres and a four story multifamily apartment building managed by Uptown Rental Properties on approximately 4 acres. The property is currently zoned OG-T Office General Transportation Corridor District. An application is being submitted to request approval of a zone change to PD Planned Development District to allow the development of a Chick-fil-A restaurant with drive through service on the existing 1.47 acre parking lot. The multifamily apartment building – The Union on Taft Apartments – will remain and will be part of the proposed PD District. Concurrent Concept Development Plan and Final Development Plan approval are requested.

The proposed Chick-fil-A restaurant will be located on approximately 1.47 acres at the intersection of E. McMillan Street and Highland Avenue replacing excess existing parking spaces. Union Taft LLC plans to create 2 lots from the existing 5.5 acre parcel, and will lease the 1.47 acre future lot to Chick-fil-A. The existing Union on Taft Apartments will be on a 4.0 acre parcel and will be owned by Union Taft LLC and will continue to be managed by Uptown Rental Properties. The restaurant will be 5,020 square feet and will have 76 off-street parking spaces for employees and customers. The restaurant will have a drive through service window with two lane customer circulation on the south, east and north side of the restaurant. The site will provide vehicular stacking spaces for the drive through exceeding the zoning requirements. One driveway for Chick-fil-A will be located on E. McMillan Street near the western property line, and one driveway will be on Highland Avenue replacing the existing driveway that serves the property. The project will allow northbound left turns on Highland Avenue at the proposed driveway within the existing right of way, complying with the findings of the traffic study by Chick-fil-A approved by DOTE.

The Chick-fil-A will be one story and 20 feet 10 inches tall. The restaurant will be a combination of dark brown and light brown brick veneer, with bronze prefinished metal coping, storefront and painted accents. Chick-fil-A wall signs are proposed on the west, south and east building elevations, and a Chick-fil-A logo sign is proposed on the north elevation. A ground sign with a brick base is proposed at both the intersections of E. McMillan Street and Highland Avenue and Highland Avenue and William H Taft. The drive through pickup window will be on the north side of the building and will be covered by a bronze metal canopy approximately 31 feet by 50 feet. A special design is proposed for the drive through order point on the south to provide building street presence along E. McMillan Street. A canopy structure with privacy screening is proposed along approximately 60 feet on E. McMillan Street. The canopy will have brick base walls and columns, and a bronze metal canopy roof to match the building. Aluminum storefront infill panels will replicate the storefront design of the restaurant. A decorative metal fence is proposed along the street east of the canopy structure, and at the corner of the property. A masonry retaining wall with a 3.5 foot tall decorative metal fence on top is proposed along the Highland Avenue frontage. We believe that the proposed order canopy structure, decorative fence, and retaining wall with fence along the street frontages provide a strong street presence for this development. There will be a 5 foot wide tree lawn, 5 foot wide sidewalk, and a 6 foot

EXHIBIT C

wide landscape area along E. McMillan Street. There will be a 7 foot sidewalk and a 5 foot landscape area along Highland Avenue, enhancing the proposed canopy, fence and wall design. Approximately 16.4% of the Chick-fil-A site will be grass and landscape open space areas.

The Union on Taft Apartments (at 237 William Howard Taft Road) are part of the proposed PD District. The Union on Taft is a four story, 103 dwelling unit apartment building with 180 parking spaces that has a combination of apartment types ranging from studios to five bedroom units. The existing apartment driveway entrances on William Howard Taft Road and E. McMillan will remain. An access easement will be provided along the northern portion of the Chick-fil-A site to maintain vehicular access to the loading dock and trash compactor areas for the Union on Taft Apartments. The apartments are currently managed by Uptown Rental Properties and will continue under this management.

We believe the proposed PD District and Development Plans are appropriate for this site. To the east of the subject property across Highland Avenue is an existing Taco Bell restaurant with drive through service in the CC-P-T District. To the south of the site across E. McMillan Street is a multi-story multifamily apartment building zoned PD District. West of the subject site is an existing parking lot zoned OG-T District. North of the site across William Howard Taft Road is a combination of residential, institutional and office uses zoned OG-T District. We believe the proposed Development Plan and PD District are consistent with the current development pattern in the area and will be compatible with the mixed-use nature of the neighborhood. We believe that extra care has been provided to the site and building design for the Chick-fil-A to ensure compatibility of this project with the neighborhood.

The development team has presented the project to many neighborhood stakeholders, including the Mt Auburn Community Council on two occasions (and will do so again in February), the Mt Auburn CDC, the Corryville CDC and representatives from the University of Cincinnati.

A review of Plan Cincinnati makes few references to the Mt Auburn neighborhood relative to Neighborhood Centers and community development. It does refence Mt Auburn as having one of the highest percentages of housing units without vehicles which one could correlate to needing more services that are walkable within the neighborhood. The plan also points out under Geographic Principles that a portion of Mt Auburn is underserved by a center of activity as it does not have its own neighborhood center. The plan goes on to state Mt Auburn does have access to other neighborhood centers. There currently are no neighborhood or community plans for Mt Auburn available on the City Planning website, either completed or under preparation, for consideration.

The property owners and surrounding stakeholders are excited about the job creation and activation of a surface parking lot that the proposed project will provide. We believe the development will add needed food choices to the neighborhood. This development will provide job opportunities for the residents in the surrounding areas and represents a significant investment in an underdeveloped area on a site that has been a surface parking lot since the 1960s. We respectfully request approval of the zone change to PD District and combined Concept and Final Development Plan approval.

PERSPECTIVE VIEW



PERSPECTIVE VIEW

EXTERIOR ELEVATION FROM HIGHLAND AVENUE



PERSPECTIVE VIEW



PERSPECTIVE VIEW

APPLICABLE CODES

BUILDING CODE: Ohio Building Code (OBC), 2017 Edition PLUMBING CODE: Ohio Plumbing Code (OPC), 2017 Edition ELECTRICAL CODE: National Electric Code (NEC), 2017 Edition ACCESSIBILITY CODE: International Building Code (IBC), 2018 Edition ENERGY CODE: International Energy Code (IEC), 2017 Edition

FIRE CODE: Ohio Fire Code (OFC), 2017 Edition GAS CODE: International Fuel Gas Code (IFGC), 2018 Edition

PLUMBING FIXTURE COUNTS

TOTAL BUILDING OCCUPANT LOAD:	207
LOAD DISTRIBUTION: FEMALE	50% MALE AND 50%
DISTRIBUTION COUNT:	104 MALE AND 104 FEMAL
LAVATORIES REQUIRED (PER CHAPTE	ER 4 IPC AND CHAPTER 29 IBC):
MALE LAVATORIES: FEMALE LAVATORIES:	104 / 200 = 1 104 / 200 = 1
LAVATORIES PROVIDED:	
MALE LAVATORIES: FEMALE LAVATORIES:	2 2

MALE WATER CLOSETS: FEMALE WATER CLOSETS:	104 / 75 = 2 104 / 75 = 2
WATER CLOSETS PROVIDED:	
MALE WATER CLOSETS:	1 WATER CLOSETS
EEMALE WATER OLOGETS:	2 WATER CLOSETS

CODE SUMMARY

1.	OCCUPANCY:	A2
2.	CONSTRUCTION TYPE:	VB
3.	SPRINKLERED:	YES
4.	ALLOWABLE AREA:	24,000 SE
	ACTUAL AREA:	4,847 SF
5.	ALLOWABLE HEIGHT:	60'-0"

4
6

OCCUPANT LOAD (SE)

EGRESS SUMMARY

REQUIRED WIDTH OF EGRESS

2. WIDTH OF EGRESS PROVIDED EXIT #1 = 66" EXIT #2 = 33" EXIT #3 = 33" EXIT #4 = 45" (STAFF ONLY)

207 x 0.15 = 32* REQUIRED

TOTAL: 177" TOTAL



D D D SUP



PROJECT NOTES

 \Box

XHIBIT

5200

E. MCMILLAN & HIGHLAND AVE CORRYVILLE, OH 45219

MCMILLAN & HIGHLAND

CHICK-FIL



Α	ATTACHED CANOPY SCHEDULE SE					
Mark	Description	Count	Overall Width	Overall Depth	Tie Back Mounting (Offset From Top)	Integra
C1-C	Exterior Canopy	10	6' - 4"	1'-0"	0' - 0"	No
C3-C	Exterior Canopy	1	15' - 4"	2.6*	0' - 0"	Yes
C4-B	Exterior Canopy	2	5'-4"	4'-0"	2'-4"	Yes
C4-L	Exterior Canopy	1	28' - 4"	4'-0"	2'-4"	Yes
Grand tot	al	14				

EXTERIOR FINISHES



DISCLAIMER: THIS PANORAMA IS FO THE PROTOTYPICAL TEMPLATE AND MAY NOT REPRESENT SITE SPECIFIC CHANGES.

any manner without express written or verbal authorized project representatives. SHEET DESIGN OVERVIEW

FSR#05724

PRELIMINARY REVISION SCHEDULE

NO. DATE DESCRIPTION



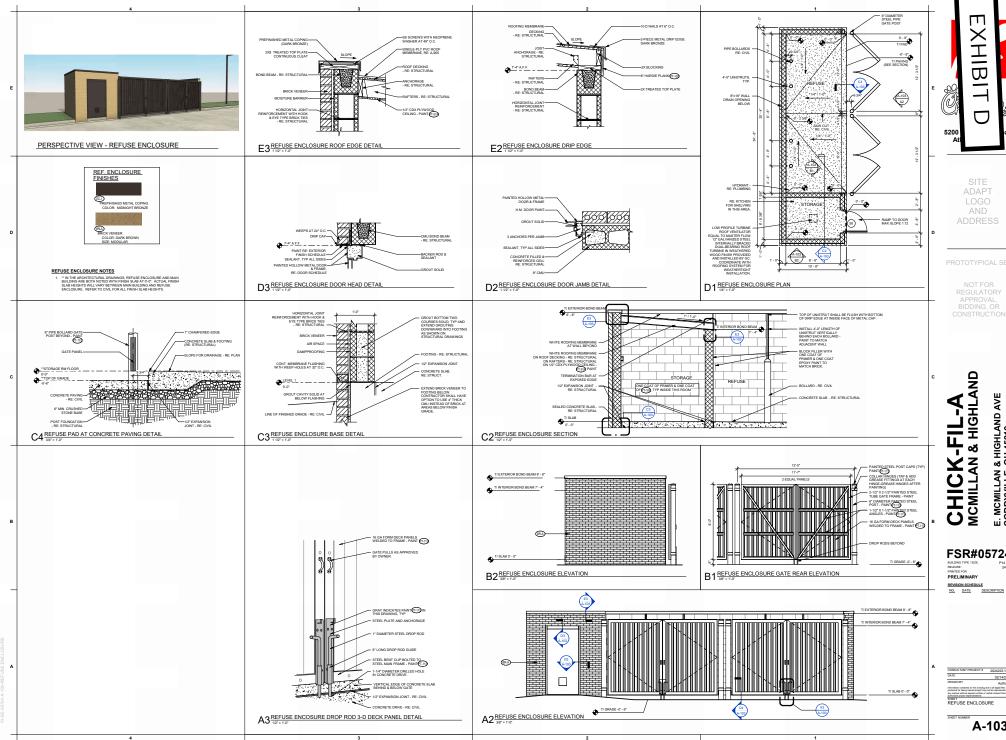


T/ SOLDIER 13' - 0" WATER TABLE 3' - 0" T/ SLAB 0' - 0"

THIGH PLATE 22 - 0"				
711.000 PARE 27 - 8 107 77.50.0.DER 19 - 07			Chick-fil:L	
N CANOPY - 8"	Q.		9	Plands and
WATER TABLE 5 - 0"				neighbors

EXTERIOR ELEVATION

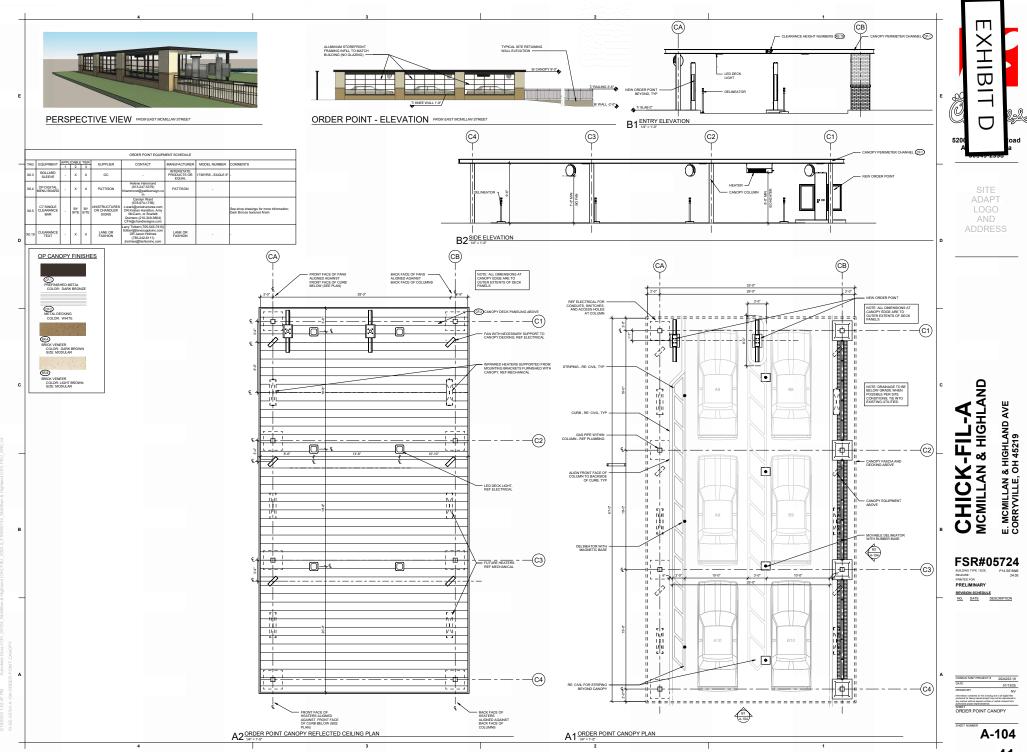
X-900



FSR#05724

A-103

40



Item	Description	Qty	Sign Area	Allowed
A1	Main ID Sign	1	72.00	247.50
A2	Shared Monument Sign	1	63.00	
В	Wall Sign - Script 5.0' (red)	1	58.75	72.00
С	Wall Sign - Script 5.0' (red)	1	58.75	72.00
D	Wall Sign - Icon 6.0'	1	36.00	
E	Wall Sign - Script 5.0' (red)	1	58.75	
F	Welcome Sign	1	26.01	
			238.26	125.81

Item	Description	Qty
K1	DOT - Handicapped Parking	3
K2	DOT - Handicapped Parking (Van)	1
L	DOT - Pedestrian Sign	2
М	DOT - Stop (30")	2
N	DOT - Stop / Do Not Enter	2
H1-H2	Menu Board (Lane 1 and 2)	2
CB-1	Clearance Bar (single) 13.00'	2
FP	Flag pole (35') High Wind	1
	Playground Graphics	1
	Vestibule Graphics	2
	Vinyl Door Graphics	1





HIGHLAND AVENUE (50' R.O.W.) EAST MCMILLAN STREET (60' R.O.W.)

GRAPHIC SCALE - 1" = 40'



5198 North Lake Drive Lake City, GA 30260 404.361.3800 www.claytonsigns.com

SITE PLAN

DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

ALL ELECTRICAL SIGNS ARE 120 VOLTS UNLESS OTHERWISE INDICATED

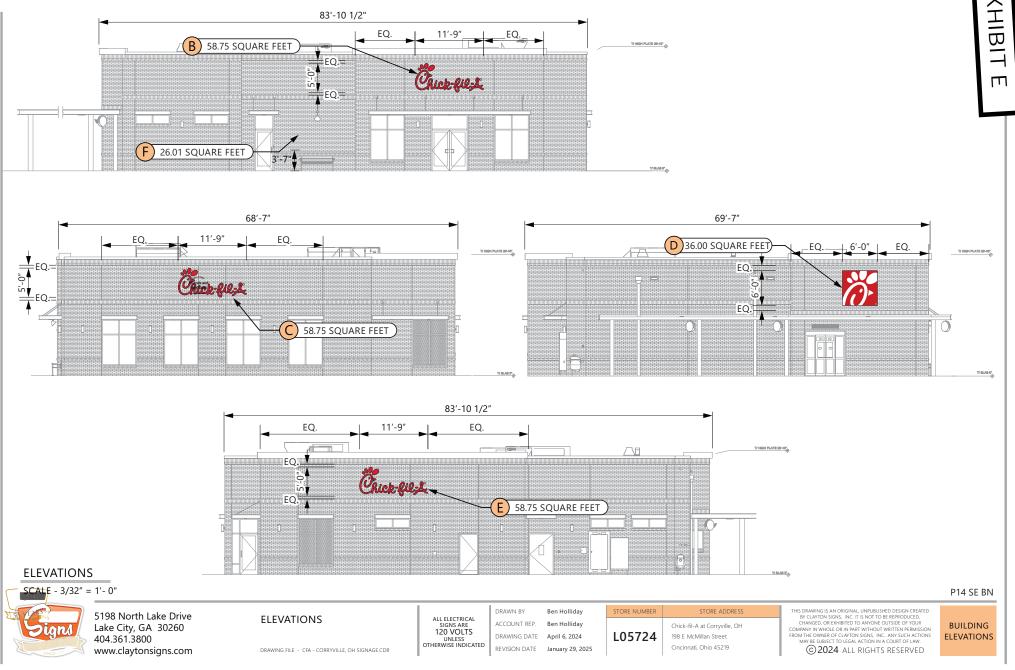
DRAWN BY DRAWING DATE April 6, 2024 REVISION DATE January 29, 2025

Ben Holliday

STORE NUMBER STORE ADDRESS Chick-fil-A at Corryville, OH L05724 198 E McMillan Street Cincinnati, Ohio 45219

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SITE PLAN







SEE ENGINEER STAMPED DRAWING FOR FOUNDATION DETAILS



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MONUMENT SIGN

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ALL ELECTRICAL
SIGNS ARE
120 VOLTS
UNLESS
OTHERWISE INDICATED

ACCI DRA

DRAWN BY Ben Holliday

ACCOUNT REP. Ben Holliday

DRAWING DATE April 6, 2024

REVISION DATE January 29, 2025

Chick-fil-A at Cornyville, OH

198 E McMillan Street
Cincinnati, Ohio 45219

STORE ADDRESS

STORE NUMBER

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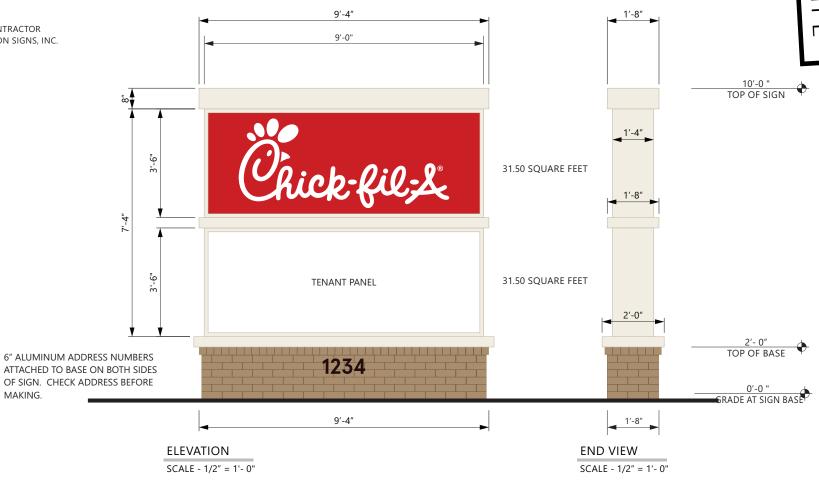
SIGN CABINET IS FABRICATED ALUMINUM CABINET WITH ACRYLIC FACES DECORATED WITH TRANSLUCENT VINYL FILM GRAPHICS.

MASONRY WORK BY THE GENERAL CONTRACTOR FOUNDATION IS FURNISHED BY CLAYTON SIGNS, INC.

COLOR TBD

WHITE ACRYLIC

MASONRY TO MATCH BUILDING





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MAKING.

SHARED MONUMENT SIGN

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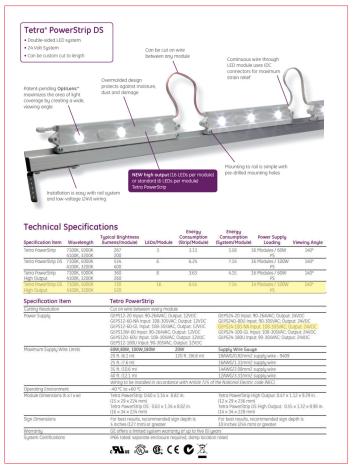
ALL ELECTRICAL SIGNS ARE 120 VOLTS UNLESS OTHERWISE INDICATED DRAWN BY Ben Holliday ACCOUNT REP. DRAWING DATE April 6, 2024 REVISION DATE January 29, 2025

L05724

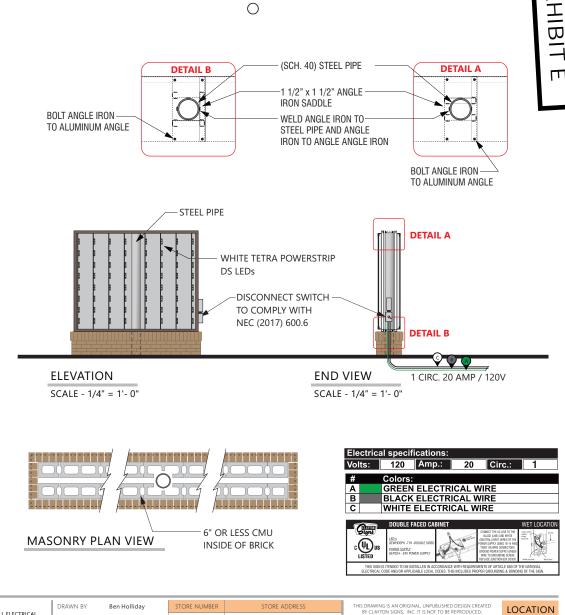
STORE NUMBER STORE ADDRESS Chick-fil-A at Corryville, OH 198 E McMillan Street Cincinnati, Ohio 45219

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DOUBLE FACED CABINET LIGHTING



THIS DRAWING IS INTENDED TO SHOW **GENERAL CONSTRUCTION DETAILS** SIZES OF CABINET AND BRICK MAY VARY





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MONUMENT SIGN DETAILS

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Chick-fil-A at Corryville, OH L05724 198 E McMillan Street Cincinnati, Ohio 45219

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A1/A2

CHICK-FIL-A SCRIPT LETTERS LETTERS ARE LED-ILLUMINATED CHANNEL LETTERS MOUNTED ON ALUMINUM SURFACE OF BUILDING WITH TRANSFORMERS REMOTELY LOCATED BEHIND THE WALL IN UL APPROVED TRANSFORMER BOXES. FACES ARE 3/16" ACRYLIC RETURNS ARE .063 ALUMINUM BACKS ARE .080 ALUMINUM ALL RETURNS ARE ARC-WELDED TO LETTER BACKS

CHANNEL LETTER FACES 2793 RED ACRYLIC TRIMCAP RETAINER-1" RED JEWELITE TRIMCAP

ALUMINUM RETURNS PAINTED TO MATCH SHERWIN WILLIAMS SW6108 LATTE



11'-9 "



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LED-ILLUMINATED **CHANNEL LETTERS**

ELEVATION

SCALE - 1/2" = 1'- 0"

DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

ALL ELECTRICAL SIGNS ARE 120 VOLTS OTHERWISE INDICATED DRAWN BY Ben Holliday Ben Holliday DRAWING DATE April 6, 2024 REVISION DATE January 29, 2025 STORE NUMBER Chick-fil-A at Corryville, OH L05724 198 E McMillan Street Cincinnati, Ohio 45219

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LOCATION B,C,E

HB

3/8" SPACER	
LETTER RETURN	П
FASTENER PER	
FIELD CONDITIONS	
SEE ENGINEERING OR BELOW.	
JUNCTION BOX (BY GC)	
LED MODULES	
SCREW-IN ROOF	:
FLEX CONNECTOR	
LED POWER SUPPLY	
INSIDE OF TRANSFORMER BOX	
20 AMP. TOGGLE	
SWITCH DISCONNECT	
FASTENER NOTE:	
USE 18-8 STAINLESS STEEL BOLTS W/ SPACERS THRU EIFS.	

EXPANSION BOLTS IN CONCRETE OR BRICK WALLS. TOGGLE BOLTS IN CONCRETE BLOCK OR PANEL WALLS. TEK SCREWS IN METAL STUDS. LAG-BOLTS IN WOOD STUDS. ALL THREAD BOLTS WITH BLOCKING BETWEEN STUDS.

CROSS-SECTION

SCALE - 1/2" = 1'- 0"

STORE ADDRESS

MASONRY WALL-

120 VOLT PRIMARY / 12 VOLT SECONDARY 3 AMPS PER SIGN PRIMARY WIRING 12 GAUGE WITH 14 GAUGE GROUND **SECONDARY WIRING 18 GAUGE**

CABINET

ALUMINUM CABINET HAS EXTRUDED ALUMINUM

FACES

FLEX FACES DECORATED WITH TRANSLUCENT VINYL

FILM ON SURFACE OF ACRYLIC.

INTERNALLY ILLUMINATED WITH WHITE LED 6500K LIGHTS.

DISCONNECT SWITCH AS REQUIRED PER NEC.

FACES

3M #3630-53 CARDINAL

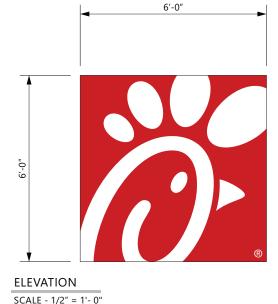
RED TRANSLUCENT VINYL

CABINET

GENESIS M SINGLE STAGE

(G2-SERIES) RED #48247

3M PANOGRAPHIC III FLEX FACE



36.00 SQUARE FEET

FASTENER NOTE:

20 AMP. SWITCH DISCONNECT

WALL SHEATHING

3/8" SPACER

3/8" STAINLESS

2 x 8" WOOD BLOCKING

JUNCTION BOX (BY GC) 2 x 8" WOOD BLOCKING

END VIEW SCALE - 1/2" = 1'- 0"

FOR MOUNTING OF SIGN

STEEL LAG BOLT INTO BLOCKING

CONDUIT FOR PRIMARY ELECTRICAL-

FOR MOUNTING OF SIGN (BY GC)

SIGN-

CABINET

USE 18-8 STAINLESS STEEL BOLTS W/ SPACERS THRU EIFS. EXPANSION BOLTS IN CONCRETE OR BRICK WALLS. TOGGLE BOLTS IN CONCRETE BLOCK OR PANEL WALLS. TEK SCREWS IN METAL STUDS. LAG-BOLTS IN WOOD STUDS. ALL THREAD BOLTS WITH BLOCKING BETWEEN STUDS.

1,-0″

CROSS-SECTION

SCALE - 1/2" = 1'- 0"

120 VOLT PRIMARY / 12 VOLT SECONDARY 3 AMPS PER SIGN PRIMARY WIRING 12 GAUGE WITH 14 GAUGE GROUND SECONDARY WIRING 18 GAUGE





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WALL SIGN

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L05724

STORE NUMBER STORE ADDRESS Chick-fil-A at Corryville, OH 198 E McMillan Street Cincinnati, Ohio 45219

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LOCATION D

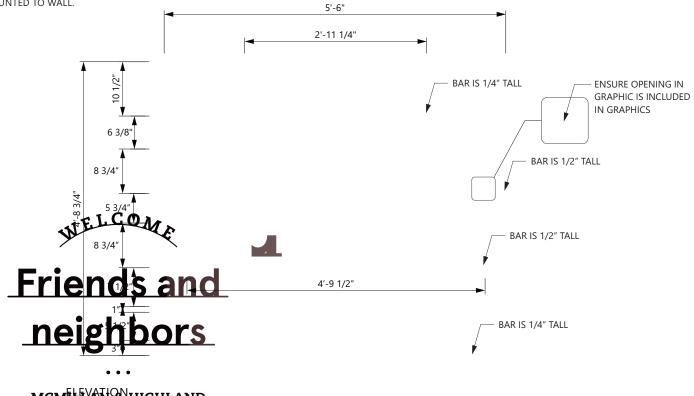
 \Box

П

FLAT CUT-OUT ALUMINUM LETTERS, 1/2" THICK, PIN-MOUNTED TO WALL.

COLORS

MATTHEWS PAINT **EQUUS BRONZE METALLIC** MP20181



MCMFFFWATON HIGHLAND SCALE - 1" = 1'- 0"

FONTS -APERCU BOLD **CAECILIA COM 85 HEAVY**



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WELCOME WALL SIGN **DIMENSIONAL GRAPHICS**

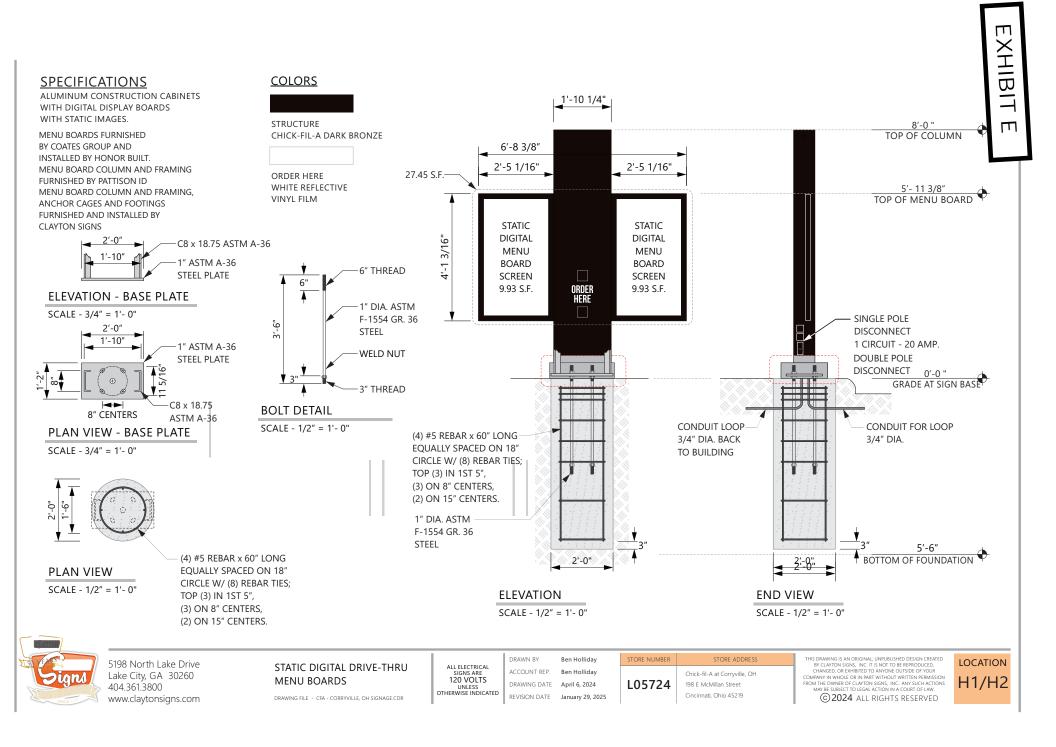
DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

ALL ELECTRICAL SIGNS ARE 120 VOLTS UNLESS OTHERWISE INDICATED

Ben Holliday DRAWN BY ACCOUNT REP. Ben Holliday DRAWING DATE April 6, 2024 REVISION DATE January 29, 2025

STORE NUMBER STORE ADDRESS Chick-fil-A at Corryville, OH L05724 198 E McMillan Street Cincinnati, Ohio 45219

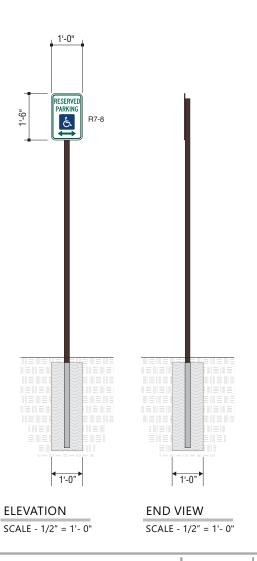
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SIGN PANELS

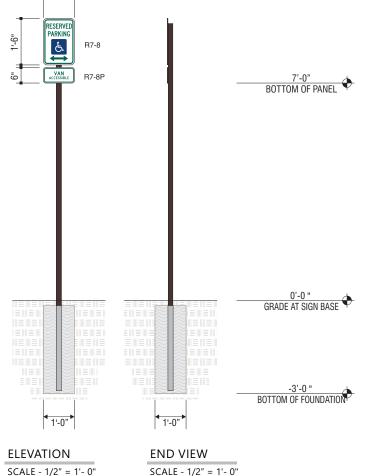
PANELS ARE .080 ALUMINUM
WITH REFLECTIVE BACKGROUND
AND GRAPHICS.
SIGN POSTS
POSTS ARE 2" x 2" SQUARE
ALUMINUM TUBING CAPPED ON TOP

BACK OF SIGN PANEL AND POST MP 20181 DARK BRONZE SW #DDM460012



NOTE: IF THIS SIGN IS LOCATED ON THE SIDEWALK THE IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTO INSTALL A PVC SLEEVE INTO THE CONCRETE WHEN SIDEWALK IS POURED PRIOR TO INSTALLATION OF THE BY CLAYTON SIGNS.

RESERVED PARKING





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DOT REGULATORY TRAFFIC SIGNS

DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

ALL ELECTRICAL
SIGNS ARE
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OTHERWISE INDICATED

DRAWN BY Ben Holliday

ACCOUNT REP. Ben Holliday

DRAWING DATE April 6, 2024

REVISION DATE January 29, 2025

STORE NUMBER STORE ADDRESS

Chick-fil-A at Corryville, OH

198 E McMillan Street
Cincinnati, Ohio 45219

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SIGN PANELS

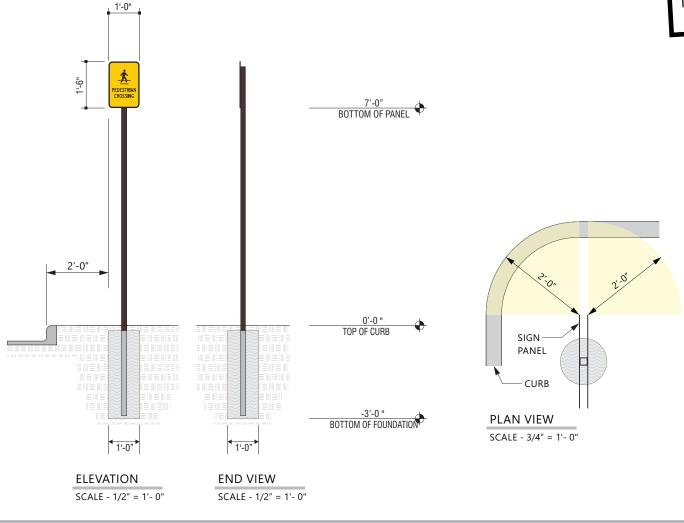
SIGNS PANELS
PANELS ARE .080" ALUMINUM. BACKS TO BE PAINTED
TO MATCH POSTS.

POSTS ARE 2" x 2" SQUARE ALUMINUM TUBING CAPPED ON TOP

BACK OF SIGN PANEL AND POST MP 20181 DARK BRONZE SW #DDM460012

SIGN PANEL (IF SHOWN)
BLACK REFLECTIVE

SIGN PANEL (IF SHOWN)
3M 680-81 YELLOW REFLECTIVE





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DOT REGULATORY TRAFFIC SIGNS

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ACCOUNT REP. Ben Holliday

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SIGN PANELS

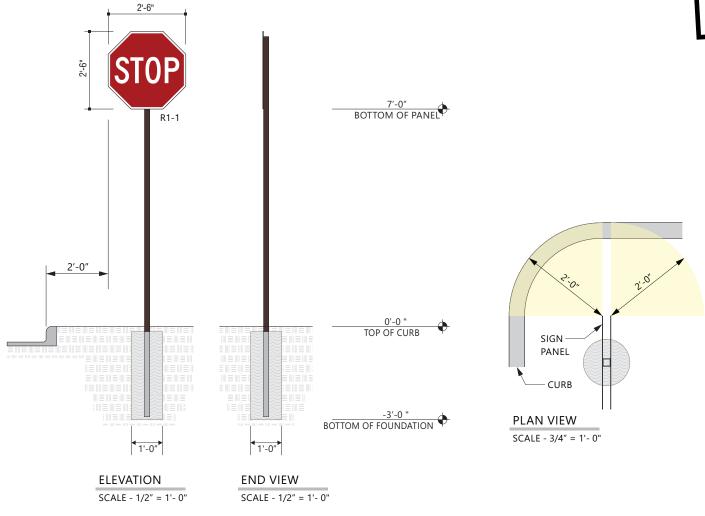
PANELS ARE .080 ALUMINUM
WITH REFLECTIVE BACKGROUND
AND GRAPHICS.
SIGN POSTS
POSTS ARE 2" x 2" SQUARE
ALUMINUM TUBING CAPPED ON TOP

SIGN PANEL

3M #680-82 REFLECTIVE RED.

SIGN PANEL
3M #680-10 REFLECTIVE WHITE

BACK OF SIGN PANEL AND POST MP 20181 DARK BRONZE SW #DDM460012





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DOT REGULATORY TRAFFIC SIGNS

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SIGN PANELS

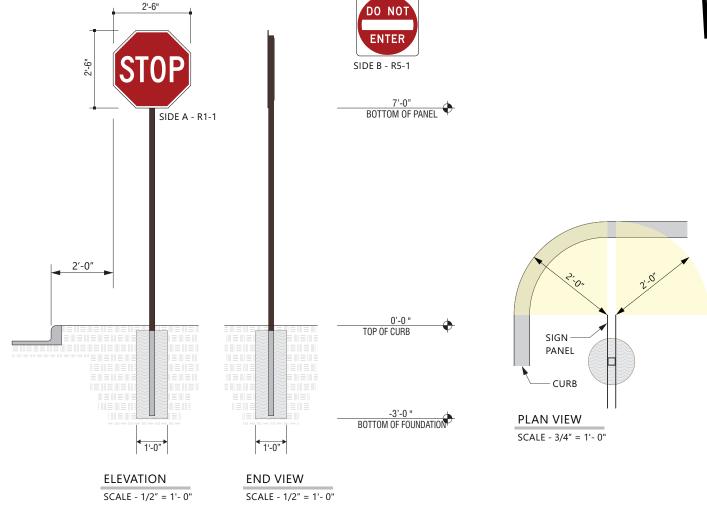
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SIGN PANEL

3M #680-82 REFLECTIVE RED.

SIGN PANEL
3M #680-10 REFLECTIVE WHITE

BACK OF SIGN PANEL AND POST MP 20181 DARK BRONZE SW #DDM460012





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DOT REGULATORY TRAFFIC SIGNS

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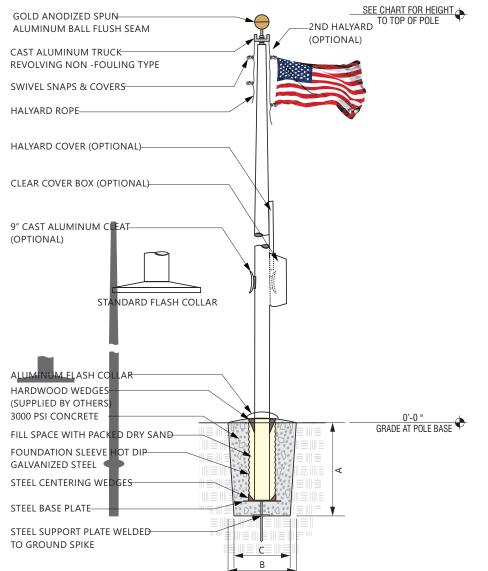
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FLAG SIZES

POLE	Α	В	MATERIAL
20'	3′0″	5′0″	POLYESTHER
25'	4′0″	6′0″	POLYESTHER
30'	5′0″	8′0″	POLYESTHER
35'	6′0″	10'0"	POLYESTHER
40'	8′0″	12'0"	POLYESTHER
45'	8'0"	12'0"	POLYESTHER
50'	12′0″	18'0"	POLYESTHER

FOUNDATION SIZES

HEIGHT	Α	В	С
20'	3'6"	30"	24"
25'	3'6"	30"	24"
30'	3'6"	30"	24"
35'	4'0"	36"	30"
40'	4'6"	42"	36"
45'	5'0"	48"	42"
50'	5'6"	48"	42"

GROUND SLEEVE AND CONCRETE INSTALLED BY THE GENERAL CONTRACTOR POLE INSTALLED ON PRE-INSTALLED GROUND SLEEVE BY SIGN CONTRACTOR



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FLAG POLE

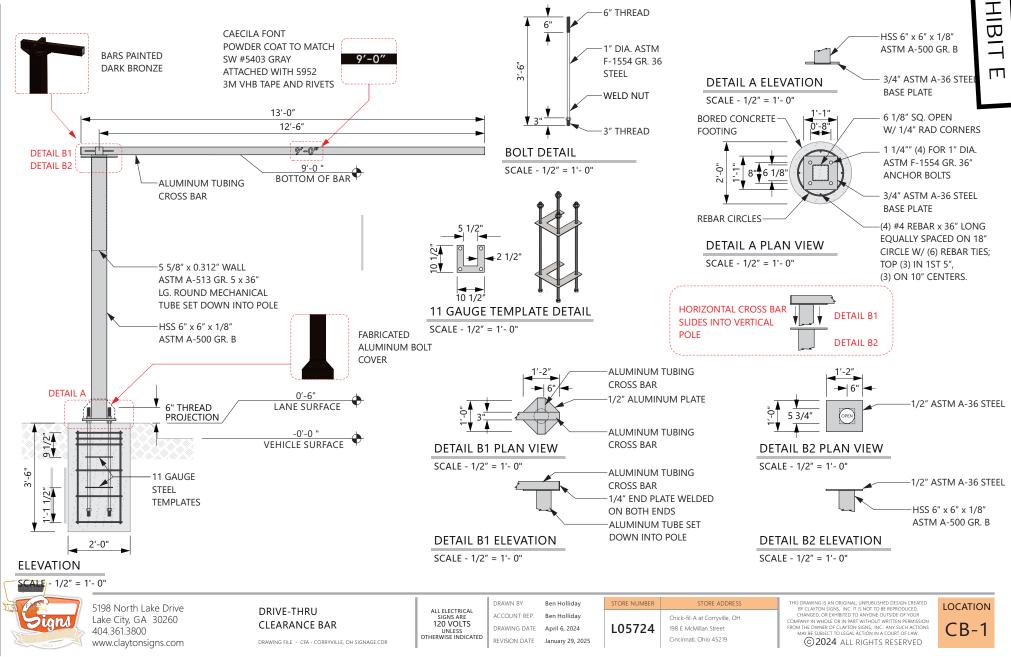
DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

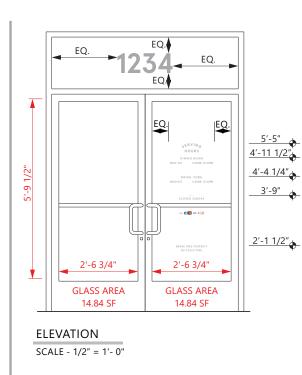
ALL ELECTRICAL SIGNS ARE 120 VOLTS UNLESS OTHERWISE INDICATED DRAWN BY Ben Holliday Ben Holliday DRAWING DATE April 6, 2024 REVISION DATE January 29, 2025 STORE NUMBER L05724

STORE ADDRESS Chick-fil-A at Corryville, OH 198 E McMillan Street Cincinnati, Ohio 45219

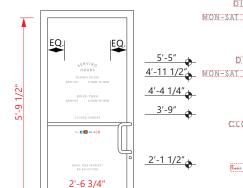
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DOOR WINDOW AREA - 14.84 SF WINDOW GRAPHIC AREA - 0.662 SF



SERVING HOURS

DRIVE-THRU

CLOSED SUNDAY

Singar WISA 180000 E

6:30AM-10:30PM

 $3.50'' \times 7.40'' = 0.18 \text{ SF}$

DINING ROOM MON-SAT 6:30AM-10:30PM

 $0.75" \times 8.50" = 0.04 \text{ SF}$

0.75" x 16.50" = 0.09 SF

 $0.75'' \times 7.90'' = 0.04 \text{ SF}$ $0.75" \times 16.50" = 0.09 \text{ SF}$

 $0.25" \times 1.47" = 0.002 \text{ SF}$ 0.75" x 9.75" = 0.05 SF

 $1.00'' \times 9.45'' = 0.07 \text{ SF}$

LETTERING IS 3M #7725-10 OPAQUE WHITE VINYL FILM (SHOWN IN GREY FOR ILLUSTRATION) CREDIT CARD LOGOS ARE INDIVIDUAL PRINTED DECALS ON CLEAR FILM. ALL GRAPHICS ARE APPLIED TO EXTERIOR SURFACE OF GLASS. STORE HOURS MAY BE DIFFERENT PER STORE.

SUNDAY HAS RED VINYL FILM STRIKE THROUGH ON IT.

PROVIDE EXTRA 6:00 PER EACH SET.

ELEVATION SCALE - 1/2" = 1'- 0"

GLASS AREA

14.84 SF

SMOKE FREE PROPERTY NO SOLICITING

 $0.75" \times 12.00" = 0.06 \text{ SF}$ $0.75'' \times 8.00'' = 0.04 \text{ SF}$

ELEVATION

SCALE - 1 1/2" = 1'- 0"

1234

0.662 SF TOTAL GRAPHIC AREA

EXTERIOR SURFACE OF GLASS. (SHOWN IN GREY FOR ILLUSTRATION) APERCU BOLD FONT

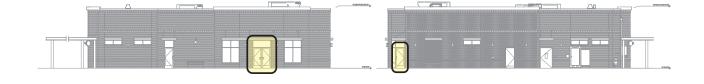
VERIFY ADDRESS BEFORE MAKING NUMBERS

8" TALL ADDRESS NUMBERS ARE 3M #7725-10 OPAQUE WHITE

VINYL FILM APPLIED TO

ELEVATION

NOT TO SCALE





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WINDOW GRAPHICS

DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

ALL ELECTRICAL SIGNS ARE 120 VOLTS OTHERWISE INDICATED

DRAWN BY

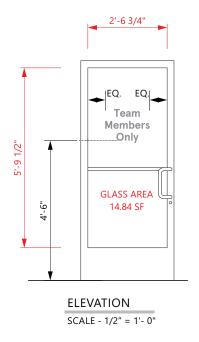
Ben Holliday ACCOUNT REP. Ben Holliday DRAWING DATE April 6, 2024 REVISION DATE January 29, 2025

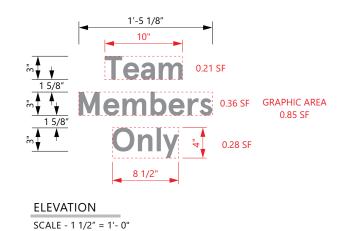
STORE NUMBER L05724

Chick-fil-A at Corryville, OH 198 E McMillan Street Cincinnati, Ohio 45219

STORE ADDRESS

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LETTERING IS 3M 7725-10
OPAQUE WHITE APPLIED VINYL
ALL GRAPHICS ARE APPLIED TO
EXTERIOR SURFACE OF GLASS.
(SHOWN IN GREY FOR ILLUSTRATION
PURPOSES ONLY)





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WINDOW GRAPHICS

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ACCOUNT REP. Ben Holliday

DRAWING DATE April 6, 2024

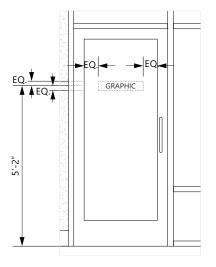
REVISION DATE January 29, 2025

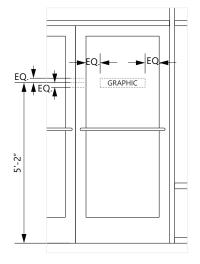
STORE NUMBER

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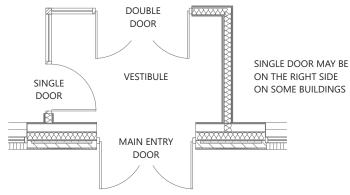


SINGLE DOOR ELEVATION

SCALE - 1/2" = 1'- 0"

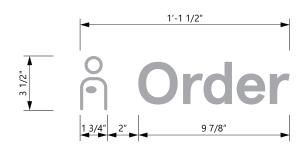


SCALE - 1/2" = 1'- 0"



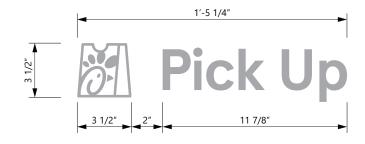


SCALE - 1/2" = 1'- 0"



DOUBLE DOOR GRAPHICS

SCALE - 3" = 1'- 0"



SINGLE DOOR GRAPHICS

SCALE - 3" = 1'- 0"

LETTERING IS 3M #7725-10 OPAQUE WHITE VINYL FILM (GRAPHICS SHOWN IN GREY FOR ILLUSTRATION)
ALL GRAPHICS ARE APPLIED TO EXTERIOR SURFACE OF GLASS.



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INTERIOR VESTIBULE DOOR GRAPHICS

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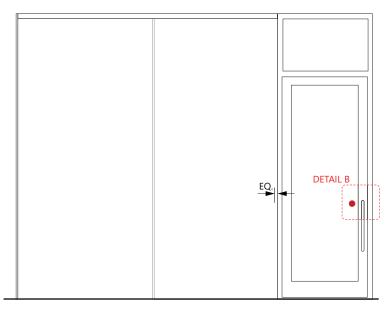
STORE NUMBER STORE ADDRESS

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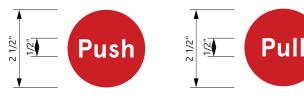
PLAYGROUND ENTRY ELEVATION

SCALE - 1/2" = 1'- 0"

ONE EACH REQUIRED - MANUFACTURE AND INSTALL DIGITALLY PRINTED GRAPHICS. FONT = APERCU BOLD APPLIED FIRST SURFACE TO DOOR AS REQUIRED PER DOOR CONDITION. EACH TO ALIGN WITH EACH OTHER ON OPPOSITE SIDES OF DOOR. DECAL IS DIGITALLY PRINTED ON 3MIJ180MC-114 CLEAR GRAPHIC FILM AND LAMINATED WITH AVERY DOL 1360Z MATTE LAMINATE. DOUBLE-SIDED PRINT WITH WHITE COPY ON FRONT



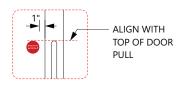
 \Box



ELEVATION

SCALE - 6" = 1'- 0"

AND SOLID RED BACK



DETAIL B ELEVATION

SCALE - 1" = 1'- 0"



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INTERIOR PLAYGROUND GRAPHICS

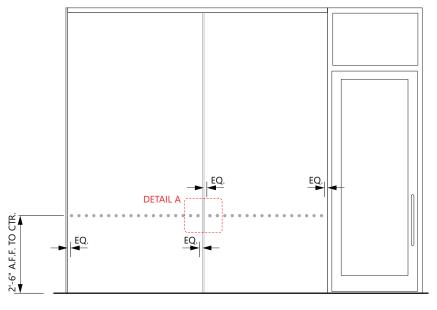
DRAWING FILE - CFA - CORRYVILLE, OH SIGNAGE.CDR

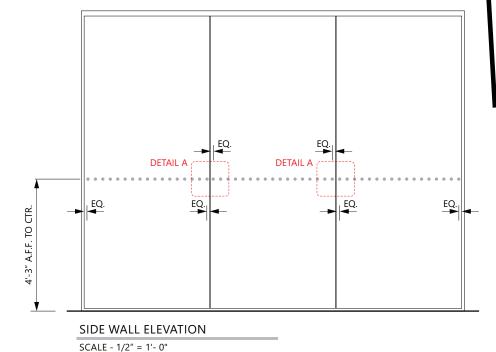
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Ben Holliday DRAWN BY Ben Holliday April 6, 2024 REVISION DATE January 29, 2025

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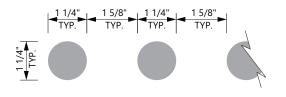




PLAYGROUND ENTRY ELEVATION

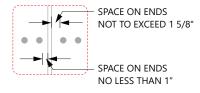
SCALE - 1/2" = 1'- 0"

VINYL FILM GRAPHICS APPLIED TO FIRST SURFACE OF GLASS
CONSISTS OF MULTIPLE DOTS IN A SERIES TO COVER WINDOWS
NEXT TO ENTRANCE TO THE PLAYGROUND. WINDOWS MAY BE ON LEFT
OR RIGHT SIDE OF ENTRANCE IN DIFFERENT BUILDINGS. SOME BUILDINGS MAY
HAVE LESS GLASS PANELS



COLOR

3M 7725-10 WHITE VINYL FILM SHOWN IN GREY FOR ILLUSTRATIVE PURPOSES



DETAIL A ELEVATION

SCALE - 1" = 1'- 0"

ELEVATION

SCALE - 6" = 1'- 0"



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INTERIOR PLAYGROUND GRAPHICS

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	REVISION HISTORY					
Date	Description					
05/14/24						
	Added current site plan.					
07/30/24	Changed prototype to SE-BS to match site plan.					
07/30/24	Discrepancies that remain:					
	The clearance bars on the civil plans need to be repos					
	Added current site plan.					
08/12/24	Added current elevations from Revit.					
	Corrected the call out for Sign A2 on the site plan.					
09/04/24	Added address to title block. Added Push Pull graphics.					
01/29/25	Added current site plan.					
	-					



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REVISION HISTORY

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EXHIBIT F

237 William H Taft and 198 E McMillan Development Schedule					
Phase I					
237 William H Taft - Union on Taft					
Office to residential conversion. 197,000 SF, 103 units					
Schedule					
April 2023	Demolition & Construction begins				
December 2023	Medical office vacates				
August, 2024	Building Opens				

Phase II					
198 E McMillan - Chick Fil A					
Current use as vacant parking lot, to 5,020 SF restaurant with drive through					
Schedule					
July 2024	Entitlement Begins				
September 2025	Construction Begins				
February 2026	Building Opens				

Zoning Information

Surveyor was not provided zoning information by client pursuant to item 6b of Table A

Items Corresponding to Schedule B-II

- 11. Reservations, restrictions, covenants, limitations, easements and/or other conditions as stated in the instrument filled for record March 25, 1947, in Deed Volume 2228, Page 569, of the Hamilton County, Ohio Records.
- 12 Grant of Easement to The Cincinnati Gas & Electric Company, filed for record April 2, 1959, in Deed Volume 3017, Page 625, of the Hamilton County, Ohio Records. Item iles within the subject property and is shown hereon.



Miscellaneous Notes

- The monuments referenced hereon (found or set) are in good condition and undisturbed (unless otherwise noted) at or near existing grade. All set pins are 5/6" wide x 30" long with an identification cap bearing the number "7911".
- MN2 The visible posted address of the subject property is 237 William Howard Taft Road.
- MN3) The subject property surveyed contains an area of 5.5079 acres (239,924 square feet), more or less.
- There are 299 regular parking spaces and 26 handicapped parking spaces, for a total of 325 parking spaces located on the subject property.
- No designation was given by the client to determine the relationship and location of certain division or party walls within respect to adjoining properties.
- At the time of survey, there was no observable evidence of earth moving work, building construction or building additions.
- MND At the time of survey, there were no changes in street right of way lines or observable evidence of street or sidewalk repairs.
- MINID Bearings shown hereon are based on the northerly Right-of-Way line of East McMillan Street, Hamilton County, Ohio. A bearing of N88*58'41"W was used.
- Subject property has direct access to William Howards Taft Road, Highland Avenue & East McMillan Street, which are dedicated, public Rights-of-Way.
- Some features shown on this plat may be shown out of scale for clarity. All dimensions shown are in feet and decimals thereof.
- MN12) At the time of survey, there was no observable evidence of site use as a cemetery or burial ground.
- At the time of the survey, there was no observed evidence of substantial areas of refuse.
- Survey prepared by: NV5, 3550 W. Market Street, Suite 200, Akron, OH 44333.

 Phone (800)787-8397, or Email Steve.Harsley@nv5.com

Flood Note

By graphic plotting only, this property is in Zone "X" of the Flood Insurance Rate Map, Community No. 380210, Map No. 39081002156 which bears an effective date of February 16, 2012 and is not in a Specia Flood Hazard Area. As shown on the FEMA website (http://mosc.fema.gov) by firmette created on March 15 2023 we have learned this community does currently articipate in the program. No field surveying was unity does currently participate in the program. No field surveying was performed to determine this zone.

	Pro	oject Rev	rision	sion Record		
Date		Description	Date		Description	
3/23/2023		lient Comments	6/27/2023		New Title	
3/30/2023	Client Comments		6/28/2023	Client Comments		
6/22/2023	_	Proposed Legal				
Elold W	fork: MG	Drofted: ACI	Chacks	d Dur D IV	ED 8 DG-	

Significant Observations

A Subject's site sign crosses into Right-of-Way by about 0.4 feet, as shown.

Legal

This survey was prepared for the purpose of this real estate transaction only and no further parties other than those certified above shall rely on it for any other purpose or transaction.

Record Description

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Situated in Section 14, Town 3, Fractional Range 2, Between The Miamis, The City of Cincinnal part of the lands of Union on Taft LLC as recorded in Official Record 14887, Page 215 of the Ha containing 5.5079 acres and being further described as follows:

Beginning at a point found by measuring from a found cross notch at the intersection of the west and with the north right of way of East McMillian Street, said cross notch being the True Point of i

thence, from the True Point of Beginning departing the west right of way of said Highland Avenu said East McMillan Street, North 83° 53' 00" West, 500.98 feet to a found 5/8' fron pin (cap illegi

thence, departing the north right of way of East McMillian Street and with a new division line th LLC, North 05' 48' 41" East, 361.30 feet to a set 5'8" iron pin on the south right of way of Willia

thence, departing the south right of way of said William Howard Taft Road and with the west right of way of said Highland Avenue, South 05' 48' 41' West, 657.54 feet to the True Point of Beginning containing 5.5079 acres of land, more or less, and being subject to all legal highways, easternets, restrictions and agreements of record.

Basis of Bearings: NAD83(2011) Ohio State Plane Coordinates, South Zone (3402).

The above description was prepared from a plat of survey made on May 2, 2023 under the direction of Jeffrey O. Lambert, Professional Surveyor #7568 in the State of Ohio.

All set iron pins are 5/8" x 30" in size with cap "BAYER BECKER"

ALTA/NSPS Land Title Survey

William Howard Taft Road Update NV5 Project No. 202300643-001 237 William Howard Taft road, Cincinnati, OH

based upon Title Commitment No. GLC2300094 of Commonwealth Land Title Insurance Company bearing an effective date of June 16, 2023

Surveyor's Certification

To: First Commonwealth Bank, a Pennsylvania banking corporation, its successors and/or assigns as their interests may appear Commonwealth Land Title Insurance Company, HCII - 237 William Howard Taft Road LLC, a Delaware limited liability company; Carton Fields P.A. and Bock & Cark Corporation, an NVS Company.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTANSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes terms 1, 2, 3, 4, 56a, 6b, 7a, 7a1, 7c, 8, 9, 10, 13, 14, 16, 17, and 19 of Table Altereol. The field work was completed on March 7, 2022.



Network Project No. 202300643-001 ICW





Transaction Services

1-800-SURVEYS (787-8397) 3550 W. Market Street, Suite 200, Akron, Ohio 44333
www.BockandClark.com maywehelpyou@bockandclark.com www.NV5.com

SURVEY - ZONING - ENVIRONMENTAL - ASSESSMENT

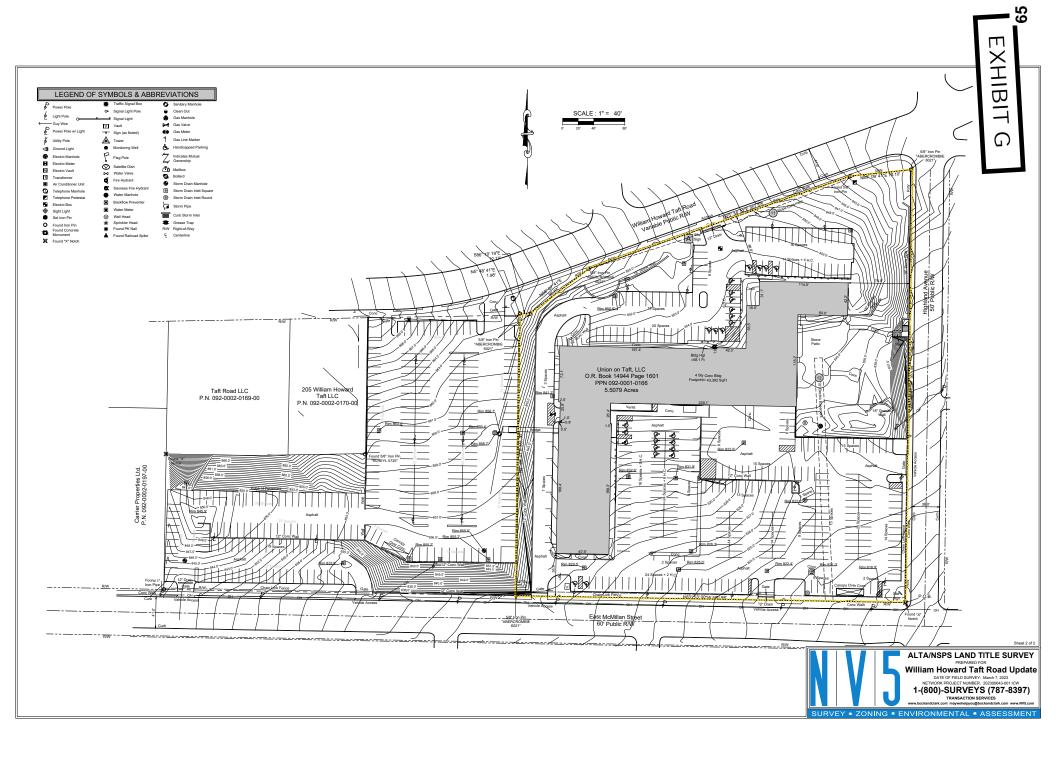


EXHIBIT H

This instrument prepared by: Carlton Fields, P.A. 4221 W. Boy Scout Blvd. Tampa, Florida 33607 Attention: Jin Liu

After recording, this instrument should be returned to: Thompson Hine LLP 312 Walnut Street, Suite 2000 Cincinnati, OH 45202 Attention: Stephen M. King

LIMITED WARRANTY DEED

THIS LIMITED WARRANTY DEED is made and entered into as of the 31st day of March, 2023 by HCII-237 WILLIAM HOWARD TAFT ROAD, LLC, a Delaware limited liability company ("Grantor"), whose mailing address is 1001 Water Street, Suite 800, Tampa, Florida 33602, to UNION ON TAFT LLC, an Ohio limited liability company ("Grantee"), whose tax mailing address is 2718 Short Vine Street, Cincinnati, Ohio 45219.

WITNESSETH:

GRANTOR, for and in consideration of Ten and No/100 Dollars (\$10.00) and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, has granted, bargained and sold, and by these presents does hereby grant to Grantee, with limited warranty covenants, the following described land situate and being in Hamilton County, Cincinnati (the "**Property**"), to wit:

SEE EXHIBIT A ATTACHED HERETO AND MADE A PART HEREOF

TOGETHER WITH all the tenements, hereditaments and appurtenances thereunto belonging or in anywise appertaining.

This conveyance, however, is made and accepted subject to the restrictions, encumbrances, easements, covenants and conditions, if any, described on Exhibit B attached hereto and incorporated herein for all purposes, to the extent that same are valid and existing and affect the Property, and any and all matters disclosed by that certain survey prepared by David J. Kuethe P.S.#7911 of Bock & Clark Corporation dated March 7, 2023, last revised March 30, 2023, Project No. 202300643-001 (collectively, the "Permitted Exceptions").

TO HAVE and to hold the same in fee simple forever.

GRANTOR hereby covenants with Grantee that it will defend the title to the Property only against the lawful claims of those persons claiming by, through, or under Grantor (other than any claim arising out of any of the Permitted Exceptions without re-imposing the same), but against no others.

Parcel No .:

092-0001-0001-00

Street address of Property: Prior Instrument Reference:

237 William Howard Taft Road, Cincinnati, Ohio

Official Records Book 12954, page 01503

Hamilton County, Ohio Recorder's Office

[Signature page follows.]



IN WITNESS WHEREOF, Grantor has caused this Limited Warranty Deed to be executed and delivered under seal by its duly authorized representative as of the date first written above.

GRANTOR:

HCII-237 WILLIAM HOWARD TAFT ROAD, LLC,

a Delaware limited liability company

By: Sila Realty Operating Partnership, LP,

a Delaware limited partnership, its Sole Member

By: Sila Realty Trust, Inc.,

a Maryland corporation, its General Partner

By:

Name: Jon Sajesk

Title: Authorized Agent

STATE OF FLORIDA)
) ss:
COUNTY OF HILLSBOROUGH)

[Notarial Seal]

Lisa A. Clarke
NOTARY PUBLIC
STATE OF FLORIDA
Comm# GG974444
Expires 4/13/2024

Notary Public, State of Florida

Print Name: Lioa A. C

My Commission Expires: 4 13 202

EXHIBIT A to Limited Warranty Deed PROPERTY DESCRIPTION

Situated in the City of Cincinnati, County of Hamilton, State of Ohio, described as follows: Situate in Section 14, Town 3, Fractional Range 2 of the Miami Purchase, City of Cincinnati, Hamilton County, Ohio, West of Highland Avenue, North of McMillan Street, South of William Howard Taft Road, more particularly described as follows:

Beginning at a recovered cross notch in a concrete walk at the northwest intersection of Highland Avenue and McMillan Street; Thence along the North line of McMillan Street, North 88°58'41" West, passing a recovered 5/8" steel rebar (PLS #6021) at 500.98, a recovered magnetic nail at 698.09 feet, a total distance of 955.32 feet to a point from which a recovered one-inch steel pipe bears North 00°59'19" East a distance of 0.33 feet to a point in the common line of the grantor and Carrier Properties, LTD (O.R. 8202 P.G. 1737); Thence leaving the North line of McMillan and with said common line North 00°59'19" East a distance of 174.37 feet to a point at the common corner of the grantor and Taft Road, LLC (O.R. 7535 P.G. 2025) from which a recovered cross notch bears North 00°59'19" East a distance of 0.23 feet; Thence with the line common to Taft Road LLC, South 89°02'41" East a distance of 113.90 feet to a point from which a recovered 1/2" steel rebar (PLS #6963) bears South 43°04'17" West a distance of 0.49 feet; Thence continuing with said line common to Taft Road LLC in part and with a line common to 205 WHT, LLC (O.R. 11599 P.G. 1717), North 89°01'19" East a distance of 143.09 feet to a point at a comer common to said 205 WHT, LLC from which a recovered 5/8" steel rebar (PLS #5724) bears South 35°00'02" West a distance of 0.86 feet; Thence with another line common to 205 WHT, LLC, North 00°53'19" East a distance of 174.95 feet to a recovered cross notch in the South line of William Howard Taft Road; Thence with said South line for six (6) calls:

North 89°01'19" East a distance of 196.13 feet to a point;

North 00°43'00" East a distance of 1.98 feet to a recovered 5/8" steel rebar (PLS #6021); North 88°44'00" East a distance of 12.17 feet to a point from which a recovered 5/8" steel rebar (PLS #6021) bears South 60°12'19" East a distance of 0.41 feet;

North 53°02'00" East a distance of 95.67 feet to a recovered 5/8" steel rebar (PLS #6021); North 70°29'11" East a distance of 357.57 feet to a point from which a recovered 5/8" steel rebar bears South 62°11'59" West a distance of 0.71 feet;

North 84°04'00" East a distance of 78.10 feet to a recovered 5/8" steel rebar at the Southwest corner of William Howard Taft and Highland Avenue;

Thence with the West line of said Highland Avenue, South 00°43'00" West a distance of 557.54 feet to the Point of Beginning.

Said parcel contains 8.161 acres.

Being all of the property to Taft Offices, LLC in Official Record 11908, Page 1384 and Official Record 11908, Page 1387 of the Hamilton County Recorder's Office at Cincinnati, Ohio. Said

herein description being the result of a field survey by Cardinal Engineering Corporation in September, 2011 under the direct supervision of Joseph G. Kramer PLS # S-8227. The bearings of this description are based on Official Record 7305, Page 2169 as recorded in the Hamilton County Recorder's Office at Cincinnati, Ohio.

EXHIBIT B to Limited Warranty Deed PERMITTED EXCEPTIONS

- 1. Rights of tenant as tenant only pursuant to that unrecorded lease with The Board of Hamilton County Commissioners for the Hamilton County Tuberculosis Control Unit as evidenced by deed filed for record December 26, 2013, in OR Volume 12496, Page 428, and the Subordination, Nondisturbance, and Attornment Agreement, filed for record December 26, 2013 and recorded in OR Volume 12496, Page 461, both of the Hamilton County, Ohio Records.
- 2. Oil and gas leases, pipeline agreements or any other instruments related to the production or sale of oil and gas which may arise subsequent to the date of the Policy, pursuant to Ohio Revised Code Section 1509.31(D).
- 3. Any lease, grant, exception or reservation of minerals or mineral rights together with any rights appurtenant thereto.
- 4. Easement for slope cuts, as stated in the instrument filed for record March 25, 1947, in <u>Deed Volume 2228</u>, <u>Page 569</u>, of the Hamilton County, Ohio Records.
- 5. Grant of Easement to The Cincinnati Gas & Electric Company, filed for record April 2, 1959, in <u>Deed</u> Volume 3017, Page 625, of the Hamilton County, Ohio Records.
- 6. Taxes and assessments for the second half of 2022 and subsequent years that are a lien but are not yet due and payable.
- 7. Zoning ordinances.
- 8. Legal highways.

EXHIBIT I



September 27, 2024

Mr. Shaun Walker Chick-fil-A 5200 Buffington Road Atlanta, GA 30349

Re: 198 E. McMillan Street | Chick-fil-A Corryville (D) – (CPRE240074) Final Recommendations

Dear Mr. Walker,

This letter is to inform you that our CSR Advisory-TEAM and CSPRO Committee has reviewed your proposed development of a 1-story fast-food restaurant and associated parking. The information provided is the recommendations of the City of Cincinnati and must be followed as you move forward with your project. As a reminder, we will have a <u>Microsoft Teams conference call meeting</u> with you on <u>October 1, 2024 @ 10 am</u> to discuss this information. Please see the feedback listed below. Thank you for developing within the City of Cincinnati.

<u>City Planning & Engagement – Planning Division</u>

Immediate Requirements to move the project forward:

- A zone change would be required for this project. Planning staff recommends a zone change to CC-M-T with additional conditions and variance relief based on a Zoning Division review. Applications for a zone change should be submitted through the Department of City Planning and Engagement. Information and materials can be found at https://bit.ly/ZoneChangePacket
- Zone change process (approximately): Public Staff Conference - 2-4 weeks City Planning Commission - 4-6 weeks. City Council - 4-14 weeks
- 3. Please set up a meeting with Planning and Zoning staff to discuss the zone change and its process.
- 4. The proposed project will require a subdivision of land. A process of determination will decide whether the project is a Minor or Major subdivision per sections 200-01-S5 and 200-01-S6 of the City's Subdivision Regulations. The subdivision may be submitted to the Department of City Planning and Engagement.
- 5. Minor Subdivisions take approximately 2-4 weeks. Major Subdivisions take approximately 8-12 weeks.

Requirements to obtain permits:

None

Recommendations:

1. The Department of City Planning and Engagement recommends that the applicant team meet with the Corryville Community Council, Mt. Auburn Community Council, and

EXHIBIT I



surrounding stakeholders. Contact information for the community council can be found here: https://bit.ly/CommunityCouncilContacts

Contact:

• Kyle Gibbs | City Planning | 513-352-4886 | kyle.gibbs@cincinnati-oh.gov

<u>City Planning & Engagement – Zoning Division</u>

Immediate Requirements to move the project forward:

- Variance for parking in the corner side yard of the proposed building (T district requires parking in the rear yard) for the row of parking along McMillan as per section 1407-04 part C of the zoning code.
- 2. Landscaping is required around the dumpster location as per Section 1421-35. This would need to be added to landscape plan.
- 3. Drive-thru is not a permitted component as part of a restaurant use in the OG. A use variance for the drive thru component would be required. A use variance requires showing that other uses permitted in the OG district are not economically feasible.
- 4. Driveways are noted as 25' wide on the site plan which is 5' more than the maximum driveway width of 20 feet. Confirmation of if these are existing driveways or new driveways is needed to determine if a variance is needed for driveway width.

Requirements to obtain permits:

- 1. Parking is conforming (no parking required in T district).
- 2. Building height is conforming.
- 3. Setbacks appear to be conforming but need to be delineated on the site plan.
- 4. FAR is conforming to the 1.75 limit. Proposed FAR of 0.08 based on the site size of approx. 63,000 SF and the building size of 5,000 SF.
- 5. Signage would be a separate permit.
- 6. All exterior light sources on private property, including canopy, perimeter, and flood, must be energy-efficient, stationary, and shielded or recessed within the roof canopy to ensure that all light is directed away from adjacent properties and public rights-of-way. The maximum height may not exceed 20 feet above grade.

Note: If re-zoned to CC-M-T, there will be several zoning relief parameters and or more information would be needed as follows:

- 7. Drive-thru is required to be on rear façade as 1409-13 and a variance would be needed. The rear façade is the west façade for the proposed parcel. It appears that the entrance is on the west façade.
- 8. Conditional use required for a drive-thru on a corner lot as per section 1409-13.
- Setback variance for the front setback as the CC-M-T section 1409-09 requires a maximum front setback of 12'. The proposed setback is greater than 12 feet to Highland (Highland is the front for zoning setback purposes).
- 10. Special exception for building placement relief as per section 1409-21 (for a corner lot location) which requires that the building be built to the front lot line if one building on the corner is built to the front lot line (building at SE corner of the intersection is built to the lot line).



- 11. Variance for parking in the corner side yard of the proposed building (T district requires parking in the rear yard) for the row of parking along McMillan as per section 1407-04 part C of the zoning code.
- 12. Additional information is needed on transparency of the south and east facades to evaluate conformance with 1409-23 as this section requires 60% transparency of the façade between 2.5' and 7' above the ground on the McMillan (south) façade and 50% transparency on the Highland (east) façade. This would be a special exception zoning relief.

Recommendations:

None

Contact:

• Wes Munzel | ZPE | 513-352-2442 | weston.munzel@cincinnati-oh.gov

Metropolitan Sewer District (MSD)

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

- 1. This site will require stormwater detention in accordance with MSD Rules Section 303.
- 2. A grease trap review will be required. Go to https://msdgc.org/customers/food-service-customers/ for information and link to online application.

Recommendations

None

Contact:

• Rob Kern | MSD | 513-244-5588 | rob.kern@cincinnati-oh.gov

Stormwater Management Utility (SMU)

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

- 1. Detention
 - If detention is required by MSD, provide SMU with a copy of the follow items: Approved detention calculations, drainage map, detailed drawing of detention control structure with elevations.
- 2. Storm Requirements
 - Calculations for storm water conveyance system, major storm calculations / flood routing.
- 3. Utility Plan
 - Label all pipes materials.
 - In the public R/W, pipes to be DIP or RCP.
 - Show Top & Invert elevations for all Appurtenances.
 - Show slopes for all pipes.
 - Show how downspouts tie to the underground sewer system.
 - Curb cuts: driveway aprons at min. 5' away from SMU inlets.
 - Tie into Curb inlets are NOT PERMITTED.
- 4. Grading Plan



- o Grading must show existing and proposed contours.
- o Impervious surfaces are NOT permitted to drain towards adjacent properties.
- Contours changes are NOT allowed to push more runoff towards adjacent properties (as compared to pre-development conditions).
- Runoff from all pavements must be captured and conveyed to the stormwater system. Only 800 sf of pavement may sheet flow to the public R/W.
- 5. Erosion & Sediment Control Plan is required. Refer to link: https://cincinnati-oh.gov/stormwater/construction-and-design/standards/sediment-and-erosion-control/
- 6. SMU Standards Plans Notes is required. Refer to link: https://www.cincinnati-oh.gov/stormwater/construction-and-design/standards/smu-standard-plan-notes-april-2022/
- 7. SMU will require an As-Built survey at the end of construction. The survey should include the following information:
 - State Plane Coordinates (N,E) for all MH's and Catch Basins.
 - o Inverts and Top elevations for all MHs and Catch Basins.
 - Slopes, sizes, and materials for all storm lines.

Recommendation:

None

Contact:

• Kevin Gold | SMU | 513-222-3643 | kevin.gold@cincinnati-oh.gov

Water Works

Immediate Requirements to move the project forward:

None.

Requirements to obtain permits:

- 1. The current water main system does meet the current fire flow requirements for commercial development. Per the direction of the Cincinnati Fire Department this development will require a sprinkler system installed. Depending on the pressure required for the sprinkler system, some water main replacement work may need to be done by the development. The available and residual pressures in this part of the water system are low and before a sprinkler system is designed the developer will need to consult with GCWW. Please contact Maria Meyer at Maria.Meyer@gcww.cincinnati-oh.gov.
- 2. Must have a stamped and recorded plat for each parcel before any building permits will be approved or water service branches sold.
- 3. Each parcel will need to have its own water service branch. Water service lines are not to cross parcel lines.
- 4. GCWW requires an outside meter pit setting followed by a backflow preventer in a heated structure.
- 5. The subject development property is receiving water service from the following:

Address	Branch #	Size	Meter #	Size	
237 William H. Taft Rd.	H-189827	6"	189827	6"	

Recommendations:

- 1. It is advised that GCWW currently has a water main construction project in the Design section of Engineering for Highland/E. McMillan/Maplewood to be constructed in 2029.
- 2. The Owner(s)/Developer(s) will need to hire a Greater Cincinnati Water Works certified licensed and bonded fire protection company and plumber to perform the private water service branch design work and installation.





3. The Owner(s)/Developer(s) must have a licensed fire protection company and plumber that is bonded and certified with GCWW and fill out the Online Branch application https://www.cincinnati-oh.gov/water/engineering-construction/forms-specifications/ for water service.

Contact:

• Rick Roell | WaterWorks | 513-591-7858 | richard.roell@gcww.cincinnati-oh.gov

Fire Department

Immediate Requirements to move the project forward:

- 1. The site plan should show 2 readily accessible Fire Hydrants within 400 feet of all sides of your project.
- 2. The minimum fire flow requirements for commercial structures is 2,000 gallons/per/minutes (GPM) @ 20 pressure/per/square inch (psi) (138Kpa).
- 3. The site plan should show the location of any proposed FDCs. A fire hydrant should be located within 50' of each FDC.

Requirements to obtain Permits:

- 1. Access Streets, Roadways or Driveways. (b) The surface shall be of sufficient strength and type to adequately support any fire division apparatus under any weather conditions.
- 2. Fire apparatus access roads shall have an unobstructed width of not less than 20 feet.
- 3. The angles of approach and departure for fire apparatus access roads shall be within the limits established by the fire code official based on the fire department's apparatus.
- 4. The weight of our apparatus is as follow:

Apparatus	Width	Length	Height	Weight	Front axle	Rear axle	Turn radius inside/outside
Ladder	10'	41'10"	11' 9"	70,000	21,600	48,000	35.45/39.25
Engine	10'	31'6"	9'5"	44,000	20,000	24,000	34'6"/41'6"
Ambulance	9' 4"	22'2"	9' 2"	18,500			34'/41'

Recommendations:

1. The Fire Department Connection can be on the building face within 50' of a fire hydrant.

Contact:

• Gregory M. Phelia Jr. | Fire Dept. | 513-357-7598 | gregory.pheliajr@cincinnati-oh.gov

Office of Environment and Sustainability (OES)

Immediate Requirements to move forward with project:

None

Requirements to obtain permits:

Commercial waste, including construction and demolition debris, generated during this
development project must utilize a City franchised commercial waste collection service
per Cincinnati Municipal Code Chapter 730. Additional information can be found at
https://www.cincinnati-oh.gov/oes/commercial-waste-hauler-program/.

EXHIBIT I



- If site plans require excavation or fill of quantities above 1000 cy, an environmental review will be required by OES. When completing the excavation and fill permit, identify the disposal and borrow site locations for all material. "TBD" will not be accepted. Specify if fill material will be soil or engineered fill, such as sand or gravel.
- 3. If offsite sourced fill is to be placed onsite which exceeds 1000 cy, then it must receive OES environmental approval as per City Municipal Code Chapters 1101 and 1031. A current Phase I ESA performed to the ASTM Standard E1527-21 of the proposed offsite borrow source property must be submitted for review. Stockpiled soils that will be used as fill material from an offsite borrow source require environmental sampling and analysis and the results be submitted for review. Please contact OES for a recommended sampling plan.

Recommendations:

- 1. The following recommendations are based on adopted City of Cincinnati environmental and sustainability policies and help to achieve the goals of the Green Cincinnati Plan:
 - a. The development goal should be to earn at a minimum the LEED Certified rating level.
 - b. Rooftop solar should be considered in the design as a renewable energy source.
 - c. Site parking should include electric vehicle charging stations.
 - d. Site areas designated for trash dumpsters should also have at least equal space designated for recycling dumpsters.
 - e. The use of trees in the landscape design should be included to enhance urban forestry.
 - f. The use of pervious surfaces should be maximized to the extent practical in the design.
 - g. Landscape design should consider the use of native species.
 - h. The use of heat reflective surfaces in paved parking areas should be considered to reduce the heat-island effect.

Contact:

• Amanda Testerman | OES | 513-352-5310 | amanda.testerman@cincinnati-oh.gov

Parks Department (Urban Forestry)

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

None

Recommendations:

None

Contact:

• Doug Fritsch | Urban Forestry | 513-861-9070 | douglas.fritsch@cincinnati-oh.gov



<u>Department of Transportation & Engineering (DOTE)</u>

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

- 1. 10' of right of way is needed on McMillan and Highland. The sidewalk can be 5' wide with a 4.5' tree lawn or 10'.
 - Install sidewalk assuming a 6" curb. The cross slope is 2%.
- 2. A Traffic Analysis for trip generation and trip distribution will be needed.
- 3. Remove unused driveway aprons and restore them to City standards.
- 4. Curb is granite on McMillan and Highland. It needs to be replaced/repaired with granite.
- 5. The maximum driveway width is 24'.
- 6. The driveways and aprons are to meet City standards. Driveway aprons can be modified for commercial (no ramps).
- 7. There need to be 2 curb ramps at the corner.
- 8. All work in the public right-of-way will require a separate DOTE permit.
- 9. Based on the site plan provided, the assigned address for the project is 198 E MCMILLAN ST. Once the building has been constructed, the address number must be posted and be visible from the street, per the Ohio Fire Code and the Cincinnati Municipal Code. Contact DTEaddress@cincinnati-oh.gov with questions.

Recommendations:

None

Contact:

• Morgan Kolks | DOTE | 513-335-7322 | morgan.kolks@cincinnati-oh.gov

Buildings & Inspections – Buildings

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

- 1. Provide a Geo-technical report at time of building permit submittal.
- 2. Code Analysis needed at time of building permit submittal.

Recommendations:

None

Contact:

• Art Dahlberg | B&I Plans Exam | 513-352-2424 | art.dahlberg@cincinnati-oh.gov

Law Department

Immediate Requirements to move the project forward:

- 1. No requirement at this time.
- 2. If this development were to create an encroachment in City right of way or property, a permanent change in the use of City right of way or City property or would require additional right of way to be dedicated, a Coordinated Report will be required. Application for Coordinated Report can be requested at real.estate@cincinnati-oh.gov.

Requirements to obtain permits:



None

Recommendations:

None

Contact:

Renee Bunch | Law | 513-352-3338 | renee.bunch@cincinnati-oh.gov

Department of Community & Economic Development (DCED)

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

None

Recommendations:

None

Contact:

• Robert Bertsch | DCED | 513-352-3773 | robert.bertsch@cincinnati-oh.gov

Health Department

Immediate Requirements to move the project forward:

None

Requirements to obtain permits:

A food facility plan review by the Cincinnati Health Department (CHD) will be required if
future or current commercial space (or tenant) is licensable (or is currently licensed) as a
food service operation (FSO) or retail food establishment (RFE). If licensable, plumbing
will not issue permits until CHD has completed the food facility review and approved the
project.

Recommendations:

1. For assistance in determining whether the facility is licensable as an FSO/RFE or not, please contact me (Trisha Blake) at the information listed below.

Contact:

Trisha Blake | Health Dept. | 513-352-2447 | trisha.blake@cincinnati-oh.gov

Police Department

Immediate Requirements to move the project forward:

None currently.

Requirements to obtain permits:

No comments.

Recommendations:

None

Contact:

- Katalin Howard | Police Dept. | 513-352-3298 | katalin.howard@cincinnati-oh.gov
- Brandon Kyle | Police Dept. | 513-564-1870 | brandon.kyle@cincinnati-oh.gov





FINAL ACTION: The CSR Advisory-TEAM and CSPRO Committee believes that the proposed project plans are moving in the appropriate direction and recommends that the project move forward to City Planning Commission subject to the following condition.

• The subject development must follow the requirements listed above to ensure that the development meets the requirements of all agencies as they apply for all permits.

Rodney D. Ringer,

Development Manager

Sincerely,

Art Dahlberg,

Director of Buildings and Inspections Department

& CSPRO Committee Chair

AD:RDR:hs

9

TRAFFIC ANALYSIS REPORT

FOR PROPOSED CHICK-FIL-A

HIGHLAND AVENUE, CINCINNATI, OHIO

Prepared For: Woolpert 4454 Idea Center Boulevard Dayton, OH 45430

Prepared By:
Jamal Adhami, PE, PTOE
SHA Engineering, LLC
January 30, 2025
Updated February 20, 2025



EXHIBIT J

Table of Content Table of Contentii 2. Scope of Services.......4 3. Existing Conditions4 4. 5. 6. **Figures** Figure 2 – 2024 Recorded /2025 No Build Traffic5 Figure 4 – Estimated New Trips8 Figure 5 - Pass-by Trips (Percentages)......9 **Tables** Table 1 - Summary of Estimated Trips6 Table 3 – Signalized/Unsignalized Intersection LOS Criteria (Exhibits 19-8, 20-2, 22-8 HCM)12 Table 5 – Capacity Analysis – Midday/PM Peak – 2025 Build – East McMillan Street at Drive B 13

List of Appendices

Appendix A - Site Plan

Appendix B - Memorandum of Understanding

Appendix C - Traffic Data

Appendix D - Turn Lane Warrants Appendix E - Capacity Analysis

1. Introduction

The purpose of this Traffic Analysis Report (TAR) is to:

- Describe and measure the impact of traffic generated by the proposed Chick-fil-A on the adjacent existing public street system; and
- Provide a list of conclusions and recommendations related to traffic operations and analysis of the proposed access drives on Highland Avenue and East McMillan Street.

The new Chick-fil-A is proposed at the northwest quadrant of the intersection of Highland Avenue and East McMillan Street in Cincinnati, Ohio. Chick-fil-A store with 5,020 SF is proposed with two access locations. The first access is proposed on Highland Avenue at the north end of the development. The second access drive is proposed on East McMillan Street near the west end of the development. Access Drive (Drive A in Figure 1) on Highland Avenue is proposed with all movements allowed. East McMillan is one-way serving eastbound traffic. Therefore, access on McMillan Street is proposed with left turn in and left turn out only. The location of the site and access locations are shown in Figure 1, and the preliminary site plan is included in Appendix A.



Figure 1 – Site Location

Study Area Access Locations

A – Access Drive A on Highland Avenue B- Access Drive B on East McMillan Street N

2. Scope of Services

SHA Engineering prepared a Memorandum of Understanding (MOU) and submitted it to City of Cincinnati on October 15, 2024. No additional comments were received from the city on MOU. A copy of the MOU is attached in Appendix B.

3. Existing Conditions

East McMillan Street in the vicinity of the site is one way eastbound only with three lanes. Roadside parking is allowed on East McMillan Street, west of the location of Drive B. It is posted at 35 mph.

Highland Avenue is carrying two-way traffic in northbound and southbound directions. It is posted at 30 mph. In the vicinity of the proposed access drive, Highland Avenue operates with 4 narrow lanes (about 9' wide).

The turning movement counts were completed at the intersection of Highland Avenue and East McMillan Street. The counts were completed October 22, 2024. The counts were used to calculate through volumes on East McMillan Street and Highland Avenue in the vicinity of the proposed access drives. Based on the recorded traffic counts, the Midday Peak Hour was recorded from 12:30 PM to 1:30 PM and the PM Peak Hour was recorded from 3:30 PM to 4:30 PM.

The observed 2024 Midday and PM Peak Hour traffic counts are shown in Figure 2.

The traffic data is included in Appendix C.

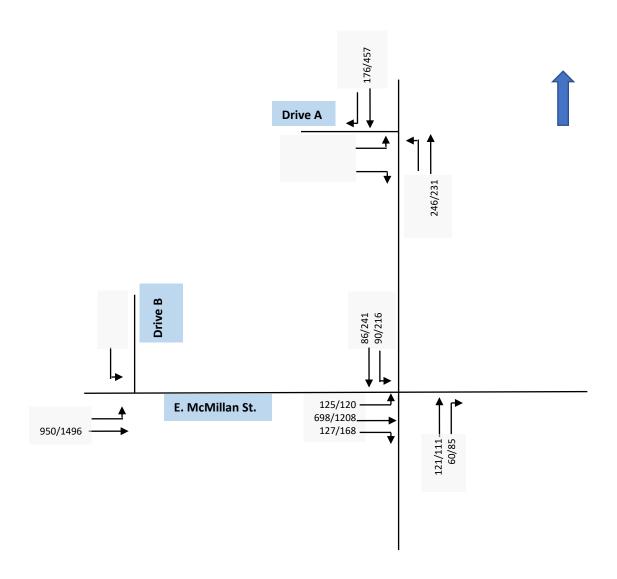


Figure 2 – 2024 Recorded /2025 No Build Traffic – Midday/PM xxx/xxx/

4. Proposed Development

The new Chick-fil-A is proposed at the northwest quadrant of the intersection of Highland Avenue and East McMillan Street in the city of Cincinnati, Ohio. Chick-fil-A store with 5,020 SF is proposed with two access locations. The first access is proposed on Highland Avenue at the north end of the development. The second access drive is proposed on East McMillan Street near the west end of the development. Access Drive (Drive A in Figure 1) is proposed Highland Avenue will allow all movements. East McMillan is operating eastbound one-way. Therefore, access on McMillan Street is proposed with left turn in and left turn out only.

The site plan is attached in Appendix A.

5. Trip Generation and Distribution

SHA Engineering completed trip generation calculations for the proposed Chick-fil-A site using the rates identified in Palmer Study referenced in the MOU. The summary of estimated trips is included in Table 1.

Table 1 - Summary of Estimated Trips									
Land Use	Land Use Unit		М	Midday Peak			PM Peak		
Lana Ose			Enter	Exit	Total	Enter	Exit	Total	
	SF	5,020	205	197	402	154	147	301	
Chick-fil-A	Pass-by		101	101	202	75	75	150	
	Primary		104	96	200	79	72	151	

The existing traffic counts were used for estimating the trip distribution of the new trips. The trip distribution for the new development is based on the following assumptions. Table 2 shows trip distribution used in the study.

Tab	Table 2 - Directional Assignment of Estimated Vehicular Trips				
	Midday/PM Peak				
	From/To				
North	30%				
South	20%				
West	30%				
East	20%				

Figure 3 shows the new trip distribution percentage, and the estimated new trips are shown in Figure 4. Pass-by Trip percentages are shown in Figure 5 and estimated Pass-by Trips are shown in Figure 6. Figure 7 shows 2025 Build traffic volumes.

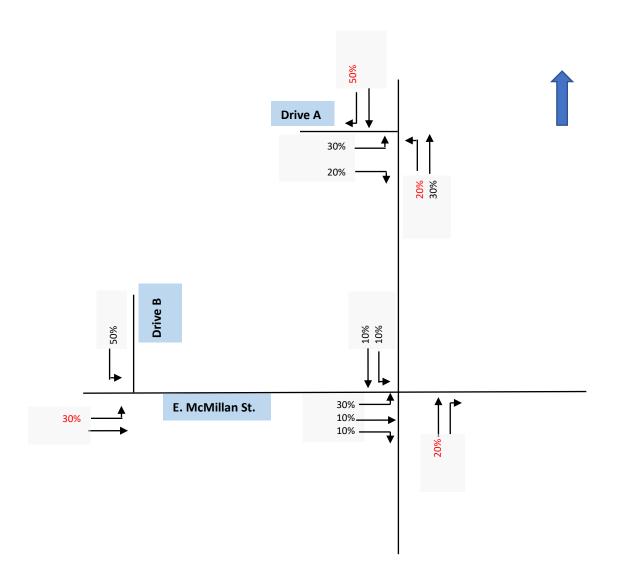


Figure 3 – Trip Distribution
(Percentages)
xx% Entering Trips
xx% Exiting Trips
Midday/PM xxx

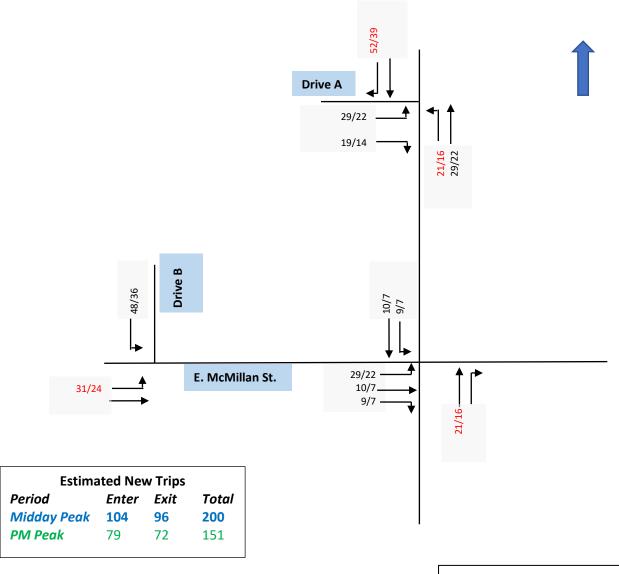


Figure 4 – Estimated New Trips
xxx/xxx Entering Trips
xxx/xxx Exiting Trips
Midday/PM xxx/xxx

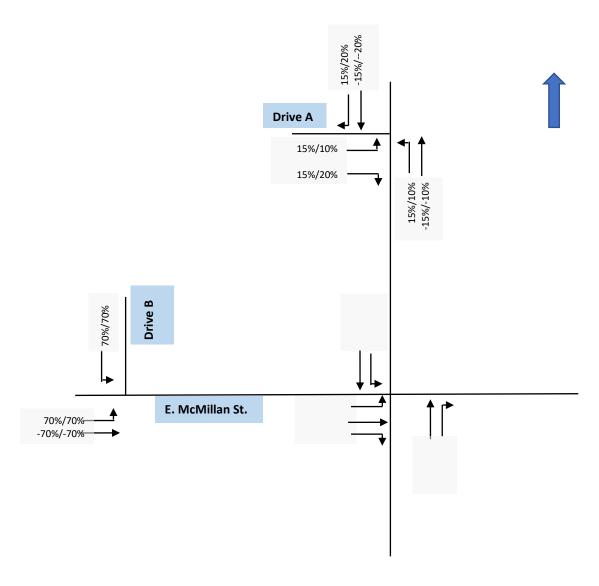
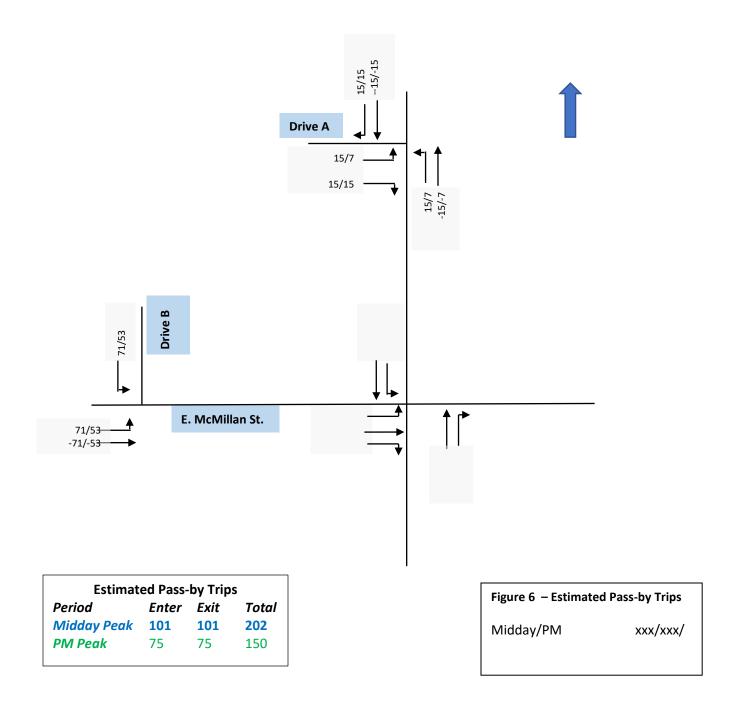


Figure 5 – Pass-by Trips (Percentages)

Midday/PM xxx/xxx/



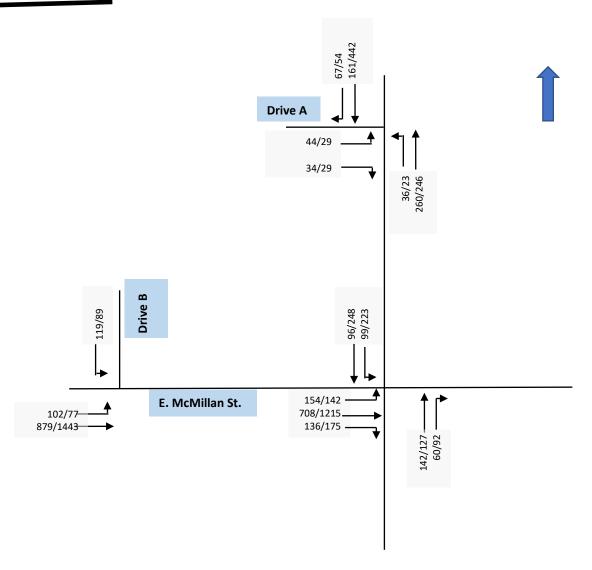


Figure 7 – 2025 Build Traffic

Midday/PM xxx/xxx/

6. Analysis

Turn Lane Warrants

The turn lane warrants were completed using the procedure identified in ODOT Location and Design Manual, Volume 1. The turn lane warrants were analyzed at Drive A on Highland Avenue. The results of the turn lane warrants are summarized below, and detailed warrants are included in Appendix E.

Highland Avenue at Drive A

- The northbound left turn lane from Highland Avenue to westbound on Drive A is warranted.
- The southbound right turn lane from Highland Avenue to westbound on Drive A *is not warranted*.

East McMillan Street at Drive B

East McMillan Street operates with 3 lanes for the eastbound traffic at Drive B. An exclusive lane for the eastbound left turn lane, with no opposing traffic, is not required.

Capacity Analysis

Capacity analyses were performed for the two access locations for the Chick-fil-A. All the analyses were completed for the Build traffic using Highway Capacity Software (HCS). Capacity of an intersection is quantified by the Level of Service (LOS) which is based upon the amount of delay a vehicle experience while at an intersection. The criterion for both signalized and unsignalized intersections (including roundabouts) are listed below in Table 3 as defined in Chapter 19, 20 and 22 of the Highway Capacity Manual (6th edition) Volume 3.

Table 3 – Signalized/Unsignalized Intersection LOS Criteria (Exhibits 19-8, 20-2, 22-8 HCM)							
Level of Service	Control Delay - Signalized Intersections (seconds/vehicle)	*Control Delay – Unsignalized Intersections (seconds/vehicles)					
A	0-10	< 10					
В	>10-20	>10 - 15					
C	>20 – 35	>15 – 25					
D	>35 - 55	>25 – 35					
E	>55 – 80	>35 – 50					
F	>80	>50					

^{*}If v/c ratio exceeds 1, LOS F

The following is a list of code definitions that are used in the capacity analysis results:

EB/WB/NB/SB-East bound/West bound/Northbound/Southbound/Northbound/Southbound/Northbou

L – Left Turn Movement (exclusive left-turn lane or lanes)

T – Through Movement (exclusive through lane or lanes)

R – Right Turn Movement (exclusive right turn lane or lanes)

EXHIBIT J

LT- Shared left turn and through movement lane

LTR – This provides movements in all directions.

TR – Shared through and right turn movement lane.

The summary of the capacity analysis completed for the intersections in the study area are included below and the results of the capacity analysis are included in Appendix F.

Highland Avenue at Drive A

The intersection will operate with stop control on the eastbound approach on Drive A. The results of capacity analysis are summarized in Table 4.

The analysis completed shows LOS B for the eastbound approach on Drive A. The northbound left turns show LOS A.

Table 4 – Capacity Analysis – Midday/PM Peak – 2025 Build – Highland Avenue at Drive A

	2025 Midday Peak Build (Un- Signalized)				2025	2025 PM Peak Build (Un-Signalized)				
	LOS	Delay (Sec/veh)	v/c	QSR	95 th %ile queue (ft.)	LOS	Delay (Sec/veh)	v/c	QSR	95 th %ile queue (ft.)
NBL	A	7.8	.03		3	A	8.6	.02		3
EBLR	В	11.4	.13		10	В	13.5	.09		8
EB Approach	В	11.4				В	13.5			
Intersection		N/A					N/A			

East McMillan Street at Drive A

The intersection operates with eastbound one-way traffic on McMillan Road and left turn out only movement allowed at Drive B. The intersection will operate with 3 lanes on East McMillan Street. The southbound left turns from Drive B will yield to the eastbound through movement on East McMillan Street. With three lanes on East McMillan Street, the southbound right turn will operate without significant delay. The summary of capacity analysis is included in Table 5 below.

Table 5 – Capacity Analysis – Midday/PM Peak – 2025 Build – East McMillan Street at Drive B

	2025 Midday Peak Build (Un- Signalized)				2025	PM Peak	Build (L	Jn-Sign	alized)	
	LOS	LOS Delay v/c QSR queue (ft.)			LOS	Delay (Sec/veh)	v/c	QSR	95 th %ile queue (ft.)	
SBL	С	17.0	.30		33	С	19.1	.28		28
SB Approach	C	17.0				C	19.1			·
Intersection		N/A					N/A			

7. Findings

The preceding analysis and recommendations listed below are based on the typical procedure used for evaluating the impact of the proposed development on the adjacent roadway infrastructure and usual customary traffic engineering standards.

The results of the analysis completed at each intersection in the study area are summarized below.

Highland Avenue at Drive A

The capacity analysis completed for the intersection shows LOS B for the eastbound approach on Drive A. The turn lane warrants completed for the intersection show the northbound left turn lane from Highland Avenue to westbound on Drive A is warranted. The northbound and southbound approaches on Highland Avenue are operating with two lanes in each direction. The through traffic on Highland Avenue is less than 500 vehicles per hour for all scenarios. The capacity for a single lane section on street similar to Highland Avenue would be around 1200 to 1500 vehicles/hour. The hourly volumes for the northbound through movement on Highland Avenue at Drive A are 260 vehicles/hour and 246 vehicles/hour for the Midday and PM Peak Hours. Therefore, a single lane for the northbound through movement will be adequate at Access Drive A. *The turning from the shared lane will have no significant impact on the northbound and southbound traffic on Highland Avenue*. Therefore, the existing lane-use on Highland Avenue in the vicinity of Drive A should be maintained.

The southbound approach at the intersection of Highland Avenue and East McMillan Street was examined to check the estimated 95th percentile queues. The summary of estimated 95th percentile queues for the southbound approach are shown in Table 6. Drive A is located about 200 feet north of the intersection with East McMillan Street. The estimated queues shown in Table 3 indicate the queues for the southbound approach at the intersection of Highland Avenue and East McMillan Street will not block Drive A.

Table 6 - Estimated Queues – Highland Avenue at East McMillan Street							
	95 th Percentile Queue (Ft.)						
	Midday Peak	PM Peak					
Southbound Left	25 83						
Southbound Through	21	86					

East McMillan Street at Drive B

East McMillan Street in the vicinity of Drive B is one way carrying traffic in the eastbound direction. The left turns from East McMillan Street to northbound on Drive B can be made with no opposing traffic. Similarly, the southbound left turns from Drive B will only yield to the eastbound traffic on East McMillan Street with 3 lanes available for the movement. The left turns from Drive B can be made with minimal delays. Access Drive B will serve the exiting traffic from the site efficiently and safely.

EXHIBIT J

Recommendations

All improvements to be made as per the recommendations in this study shall be completed following Standards published by Ohio Department of Transportation and supplements to these standards followed by City of Cincinnati.

Highland Avenue at Drive A

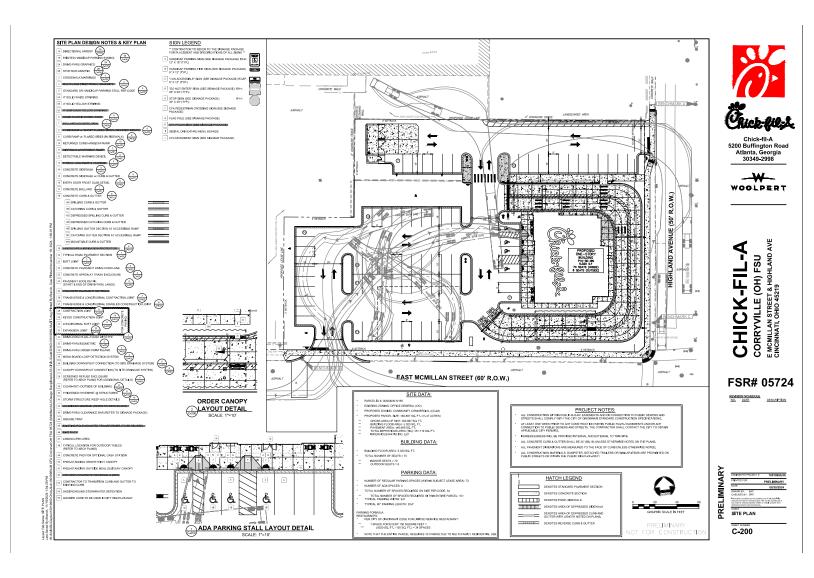
- Drive A shall be constructed with a single inbound and single outbound lane.
- A stop sign shall be installed on the eastbound approach on Drive.

East McMillan Street at Drive B

- Drive B shall be constructed with a single inbound and single outbound lane.
- Stop Sign shall be installed on the southbound approach on Drive B.

Appendix A Site Plan

EXHIBIT J



Appendix B Memorandum of Understanding



MEMORAMDUM OF UNDERSTANDING

October 15, 2024 Consultant: SHA Engineering, LLC

Submitted to

Morgan Kolb City of Cincinnati Transportation Department 801 Plum Street Cincinnati, OH 45202

RE: TIS - Proposed Chick-fil-A, Highland Avenue, Cincinnati, Ohio

Background

The scope of work included in the Memorandum of Understanding (MOU) is for completing a traffic analysis report for the proposed Chick-fil-A at the northwest quadrant of the intersection of Highland Avenue and East McMillan Street in City of Cincinnati, Ohio. Chick-fil-A store with 5,020 SF is proposed with two access locations. The first access is proposed on Highland Avenue at the north end of the development. The second access drive is proposed on East McMillan Street near the west end of the development. Access Drive (Drive A in Figure 1) is proposed Highland Avenue will allow all movements. East McMillan is operating eastbound one-way. Therefore, access on McMillan Street is proposed with left turn in and left turn out only. The location of the site and access locations is shown in Figure 1 and the preliminary site plan is included in the Appendix.

A coordination meeting was held with the City on October 10, 2024. The scope of work included in this Memorandum of Understanding (MOU) is developed based on the discussions and feedback provided by the city during the meeting on October 11, 2024.

The scope of services for the traffic impact study will include the following:

Study Area

The study area intersections are listed below and shown in Figure 1 (Location Map) included in the last page of this Memorandum of Understanding.

- Highland Avenue at Access Drive A.
- East McMillan Avenue at Access Drive B.

Traffic Counts

The turning movement traffic count data during the weekday Mid-day Peak and PM peak hours will be recorded at the intersection of Highland Avenue and East McMillan Street. The traffic counts will be recorded during the hours from 6:00 AM to 7:00 PM. The counts will be used for estimating the directional



counts on Highland Avenue and East McMillan Street in the vicinity of access locations. The counts will also be used for establishing Midday and PM Peak Hours for the analysis.

Trip Generation/Distribution

SHA Engineering completed trip generation calculations for the proposed Chick-fil-A site. Trip generation equations were referenced from the *Proposed Chick-fil-A Traffic Impact Study, Palmer Engineering, 2021* which return higher values than the Institute of Transportation Engineers (ITE) *Trip Generation Manual* equations for fast food restaurants. Based on the referenced study data, it is assumed that 50% of the trips generated will be pass-by trips. **Table 1** shows the equations used to estimate total trips for the Chick-fil-A restaurant and **Table 2** shows the trip estimated for the site.

Table 1- Growth Rate

I and Uga	Tin:4	Wee	kday
Land Use	Unit	Mid-Day Peak Hour	PM Peak Hour
Chick-fil-A	Per 1.000 SF	T = 80 (X)	T = 60 (X)
CHICK-III-A	Fei 1,000 SF	51% in/49% out	51% in/49% out

Table 2- Estimated New Trips

Land Use	Unit	Size		AM Peak			PM Peak	
Lana Ose	Onu	Size	Enter	Exit	Total	Enter	Exit	Total
	SF	5,020	205	197	402	154	147	301
Chick-fil-A	Pass	Pass-by		101	202	75	75	150
	Prim	Primary		96	200	79	72	151

The following scenarios will be developed for analysis.

- 2025 Build Traffic

Analysis

The following analyses will be completed.

Turn Lane Warrants

Turn Lane Warrants will be completed to determine if the exclusive lanes for the left and right turns are required at access drives included in the study area. The turn lane warrants will be completed for the free flow approaches of the intersections. The turn lane warrants will be completed using the procedure identified in the ODOT Location and Design Manual, volume 1.

Level of Service/Queue Analyses:

The intersections identified in the study area will be analyzed for Level of Service (LOS) and Delay using **Highway Capacity Software (HCS-2023**). Traffic operations will be evaluated during the **Weekday Midday Peak and PM peak hours** for the scenarios identified earlier in this report.



Storage Analyses:

Turn lane storage length analysis will be performed in accordance with the standards used by ODOT Figures 401-9 and 401-10 ODOT Location and Design Manual, volume 1).

Conclusions/Summary

This report will provide an evaluation of the scenario identified above and will include recommendations for improvements, if any, necessary to mitigate the impacts of the additional traffic from the proposed site and maintain an acceptable level of service and operational safety. An appendix will be included containing the traffic counts data and analysis output.

Deliverables

The Traffic Analysis Report will be completed and submitted to the city for review and approval. The study will provide a summary of the findings and will include recommendations to mitigate the impact of additional traffic generated by the development.

Figure 1 – Locadtion Map



Proposed Site



Study Area Access Locations

A – Access Drive A on Highland Avenue B- Access Drive B on East McMillan Street



Pages from Proposed Chick-fi-A Palmer Study For Columbus Site, 2021

APPENDIX

C.

Pages from Proposed Chick-fil-A Traffic Impact Study Columbus, OH, Palmer Engineering, 2021

		1				
Store Location	Time	Entering	Exiting	Total	Drive Thru Queue	
Store Location	Period	Volume	Volume	Volume	Length (Vehicles)	
Weekday (AM Peak)						
Sancus Blvd	9:15 – 10:15	64	52	116	Less than 5 Vehicles	
Tuttle Crossing	7:45 - 8:45	76	60	136	Less than 5 Vehicles	
Reynoldsburg	8:00 - 9:00	79	71	150	Less than 5 Vehicles	
Weekday (Noon Peak)						
Sancus Blvd	12:00-1:00	203	149	352	25 Vehicles, 8 Vehicles*	
Tuttle Crossing	12:00-1:00	247	216	463	21 Vehicles	
Reynoldsburg	1145 - 12:45	240	210	450	18 Vehicles	
Weekday (PM Peak)						
Sancus Blvd	4:30-5;30	158	129	287	23 Vehicles, 8 Vehicles*	
Tuttle Crossing	6:00-7:00	174	182	356	9 Vehicles	
Reynoldsburg	5:30-6:30	189	174	363	15 Vehicles	
Saturday (AM Peak)						
Sancus Blvd	8:15 – 9:15	97	72	169	12 Vehicles	
Tuttle Crossing	8:00 - 9:00	59	54	113	Less than 5 Vehicles	
Reynoldsburg	8:00 - 9:00	67	64	131	Less than 5 Vehicles	
Saturday (Noon Peak)						
Sancus Blvd	12:00-1:00	169	121	290	25 Vehicles, 11 Vehicles*	
Tuttle Crossing	11:15 – 12:15	196	169	365	9 Vehicles	
Reynoldsburg	11:45 – 12:45	202	210	412	17 Vehicles	
Saturday (PM Peak)						
Sancus Blvd	5:30 - 6:30	161	125	286	24 Vehicles, 12 Vehicles*	
Tuttle Crossing	5:15 - 6:15	174	183	357	11 Vehicles	
Reynoldsburg	4:15 – 5:15	188	158	346	13 Vehicles	
				* Denotes temporary drive thru lane		

Table 1. Chick-Fil-A Store Use Summary



Figure 1. Drive Thru Queue Simulation



Based on this data and the traffic simulation of the drive thru we are able to make sure adequate queue storage is provided at the proposed Chick-Fil-A. As part of the study we collected the amount of time a vehicle spends at the ordering station and then when they pulled up to the window to get their food. This data has been collected at multiple sites including Grand Rapids, Michigan, Richmond, Ky, and Columbus Ohio. Included in the Appendix is the data gathered for ordering and pick-up window times. This data allows us to accurately model the drive thru restaurant with a double thru and the median results of the data are that it takes 42 second for ordering and 28 seconds for the delivery of food once the vehicles stops at the window. With the COVID pandemic ongoing, the drive thru orders are at a higher than normal volume so the drive thru is modeled with only a few dine-in and pick-up orders and confirms the worst case of queueing. The current layout can queue 23 vehicles in each lane without impacting traffic flow around the site. The Sancus Blvd location currently serves about 200 entering vehicles at the highest volume and the other restaurants are serving over 240 entering vehicles during the peak period with queue length less than Sancus Blvd due to the double drive thru. The double drive thru provides significant capacity for the store and reduces the on-site queueing. This site will be able to adequately serve the increased volume if demand increases at this location.

ROADWAY TRAFFIC ANALYSIS

The flow of traffic along the roadways surrounding the Mall was also analyzed to determine if adequate capacity was provided for vehicles to enter and exit the proposed site. Manual traffic counts were taken from 7:30 AM to 9:00 AM, 11:00 AM to 1:00 PM, and 4:00 PM to 7:00 PM on November 20, 2021 and November 23, 2021 at the intersection of Polaris Parkway/Town Centre(West), Polaris Parkway/Town Centre(East), Mall Connector Road/Town Centre(East), and Mall Connector Rd/Town Centre(West). The highest hourly traffic volumes were selected for the analysis of the roadway and traffic simulation. All existing traffic volumes can be found in the Appendix and are shown on Figures 1 and 2.

The estimated completion date for the proposed development is by the end of 2022. Based on ODOT count stations the AADT along Polaris Parkway has been flat from 2018 to 2020 so traffic was not increased from the existing counts. Currently Polaris Parkway has 48,000 vehicles per day. The existing count station data can be found in the Appendix.

Methodology

Level of Service and delay were measures of effectiveness analyzed using the SYNCHRO 11 software. HCS and the HCM method does not calculate shared lanes therefore, SYCNHRO 11 uses similar methods to calculate the delay, level of service, and queue length for the signalized intersections while SYCNHRO 11 used HCM methods for calculating the same values for the stop controlled intersections.

Trips were generated for the proposed development and then distributed to the roadway system based on the existing traffic patterns and engineering judgment. For the analysis, the study uses traffic volumes from the counts that were gathered in the field. The assigned volumes from the proposed development and the background traffic volumes combined to produce the total proposed traffic volumes for existing conditions. Background traffic volumes were grown by a percentage



determined based on historic traffic volumes, for this study that was 0%. These increased volumes were considered the background traffic volumes for the design year, 2032. Generated trips were then added to the design year background traffic to determine the design year build volumes. HCS7 was used to analyze the roadway network for existing and proposed conditions in during the opening year and design year, 2022 and 2032 respectively. The background, level-of-service, and vehicle delay can be found in the Appendix along with 2022 Weekday Existing (Fig 1), 2022 Weekday Build (Fig 3), 2022 Saturday No Build (Fig 2), and 2022 Saturday Build (Fig 4) traffic volumes.

Level of Service and Delay

Level of Service (LOS) was used as the measure of effectiveness for each lane and turning movement. According to the Highway Capacity Manual, the level of service is defined in terms of delay (See Tables 2 and 3). Delay results in driver discomfort, frustration, fuel consumption, and lost travel time. Delay is caused by a number of factors including traffic signal timing, geometrics, traffic congestion, and accidents at an intersection. Level of Service is based on a grade scale from A to F with A being excellent and F being failure. A Level of Service C is desirable, and D is acceptable in an urban setting.

Table 2 – Unsignalized Intersections	
Level of Service	Delay (Seconds per Vehicle)
A	<=10
В	>10 and <=15
C	>15 and <=25
D	>25 and <=35
E	>35 and <=50
F	>50

Table 3 – Signalized Intersections					
Level of Service	Delay (Seconds per Vehicle)				
A	<=10				
В	>10 and <=20				
C	>20 and <=35				
D	>35 and <=55				
Е	>55 and <=80				
F	>80				

Trip Generation and Proposed Traffic Volumes

Trip estimates for the proposed development are based upon data provided in the *Trip Generation*, 11th Edition, a nationally recognized resource of trip generation rates published by the Institute of Transportation Engineers. The average rates that the ITE generates are less than typical Chick-Fil-A restaurants so we have adjusted that volumes to replicate data that is specific to Chick-Fil-A.



Site Trip Generation

The proposed site will consist of a Chick-fil-A restaurant store. For this study a Fast-Food Restaurant with Drive-Through Window (Code 934) was checked versus the information provided by Chick-fil-A and determined to be below the average rates. The ITE average rates generate 229 (AM Peak), 256 (Mid-Day Peak), and the 230 vehicles (PM peak). For this study we have used higher rates to provide a conservative analysis and to accurately replicate what Chick-Fil-A generates during peak hours. The rates shown on the charts below closely replicate the total number of vehicles that would visit this location based on counts that were taken at the site and are at the higher range of the ITE provided data.

As part of other traffic impact studies Palmer Engineering has counted existing Chick-Fil-A stores in Columbus, OH, Cincinnati, OH, Virginia Beach, VA, and Richmond, KY and determined that the Mid Day Peak is typically 360 vehicles (190 Entering and 170 Exiting). Based on these findings, experience, and engineering judgement it was determined that approximately 16% of the weekday trips generated occur during the Mid-Day Peak. Using 16% the average rate for the Mid-Day Peak was taken to be approximately 80.00.

	Chick-fil-	-A Propose	d Developn	nent Trip Genera	ation Tabl	e		
ITE Code	Land Use	Sq Ft/ Units	Average Rate	AM Peak	Enter	Enter %	Exit	Exit %
934	Fast-Food Restaurant with Drive-Through Window	4.48	51	229	119	52%	110	48%
	Total			229	119		110	
ITE Code	Land Use	Sq Ft/ Units	Average Rate					
				Mid-Day Peak	Enter	Enter %	Exit	Exit %
934	Fast-Food Restaurant with Drive-Through Window	4.48	80	359	183	51%	176	49%
	Total			359	183		176	
								•
ITE	Landillea	Sq Ft/	Average					
Code	Land Use	Units	Rate	PM Peak	Enter	Enter %	Exit	Exit %
934	Fast-Food Restaurant with Drive-Through Window	4.48	65	291	148	51%	143	49%
	Total			291	148		143	

Table 4 – New Trip Generation Table

This report and analysis assumes that pass-by trips will utilize the Chick-fil-A restaurant. Pass-by trips are vehicles who are already on the roadway today and will choose to stop at the new development, utilize the development's service, and then continue on their way. An example of this type of trip would be someone who currently uses this route to go to work, stops at the restaurant on the way to or from work to pick up food, and then continues on their route. This analysis assumes a 50% pass-by rate for the Chick-fil-A restaurant. This is conservative in this area since the existing store is located at the next signal and an argument could be made that all the trips are already using the roadway and just need to be redirected to the site. Table 5 shows the reduced new trips generated by the development and the remaining trips are redirected from the existing traffic on Polaris Parkway.



Chick-fil-A Proposed Development Trip Generation Table								
ITE Code	Land Use	Sq Ft/ Units	Average Rate	AM Peak	Enter	Enter %	Exit	Exit %
934	Fast-Food Restaurant with Drive-Through Window	4.48	51	132	69	52%	63	48%
	Total			132	69		63	
ITE Code	Land Use	Sq Ft/ Units	Average Rate					
				Mid-Day Peak	Enter	Enter %	Exit	Exit %
934	Fast-Food Restaurant with Drive-Through Window	4.48	80	179	91	51%	88	49%
	Total			179	91		88	
ITE	Laurel Llaur	Sq Ft/	Average					
Code	Land Use	Units	Rate	PM Peak	Enter	Enter %	Exit	Exit %
934	Fast-Food Restaurant with Drive-Through Window	4.48	65	146	74	51%	72	49%
	Total			146	74		72	

Table 5 – New Trip Generation Table with Pass-by Trips



Figure 2. Site Map

Appendix C Traffic Data

30 Intersection of Highland Avenue at Drive A SHA ENGINEERING LLC - 24054

Start Time			ve A bound				I/A bound				d Avenue abound				d Avenue abound		Total (All Approaches)
	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	
					MII			R TURNII									
						2024 C	ounted Tra	iffic Volume	es/2025 No	Build Traf	fic						
12:30 - 1:30 PM										246				176			
								New Trip	os								
12:30 - 1:30 PM	29		19						21	29					52		
								Pass-by 1	Γrips								
12:30 - 1:30 PM	15		15						15	-15				-15	15		
								2025 Build	Traffic								
12:30 - 1:30 PM	44	0	34		0	0	0		36	260	0		0	161	67		
					P			TURNING									
						2024 C	ounted Tra	ffic Volume	es/2025 No	Build Traf	fic						
3:30 - 4:30 PM										231				457			
								New Tri	os								
3:30 - 4:30 PM	22		14						16	22					39		
								Pass-by 1	Trips								
3:30 - 4:30 PM	7		15						7	-7				-15	15		
								2025 Build									
3:30 - 4:30 PM	29	0	29		0	0	0		23	246	0		0	442	54		1

30 Intersection of East McMillan Street at Drive B SHA ENGINEERING LLC - 24054

			lillan Street	ì			lillan Street				I/A				ive A		Total (All
Start Time		East	bound			Wes	bound			North	bound			South	nbound		Approaches
	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	
					M	DDAY PE	AK HOU	R TURNII	NG MOV	EMENT C	OUNTS						
						2024 C	ounted Tra	affic Volum	es/2025 No	Build Traf	fic						
12:30 - 1:30 PM		950															
								New Tri	ps								
7:30 AM- 8:30 AM	31												48				
								Pass-by	Trips								
	71	-71											71				
								2025 Build	Traffic							_	
7:30 AM- 8:30 AM	102	879	0		0	0	0		0	0	0		119	0	0		
								TURNING									
						2024 C	ounted Tra	affic Volum	es/2025 No	Build Traf	fic						
3:30 - 4:30 PM		1496															
								New Tri	ps								
3:30 - 4:30 PM	24												36				
								Pass-by	Trips								
3:30 - 4:30 PM	53	-53											53				
								2025 Build	Traffic							_	
3:30 - 4:30 PM	77	1443	0		0	0	0		0	0	0		89	0	0		

EXHIBIT J

Provided by: SHA Engineering 6221 Thomberry Court, Mason, OH, 45040, US

Tue Oct 22, 2024 Full Length (6 AM-10 AM, 2 PM-7 PM) All Classes (Lights and Motorcycles, Heavy) All Movements

ID: 1239203, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan

Leg		East McMil	lan Street			Highland A	venue			Highland A	venue			
Direction		Eastbound				Northbound	l			Southbound				
Time		R	T	L	App	R	T	U	App	T	L	U	App	
	2024-10-22 6:00AM	0	28	7	35	7	3	0	10	2	3	0	5	50
	6:15AM	5	42	14	61	10	20	0	30	5	3	0	8	99
	6:30AM	7	77	8	92	6	61	0	67	12	7	0	19	178
	6:45AM	11	76	18	105	14	41	0	55	8	10	0	18	178
	Hourly Total	23	223	47	293	37	125	0	162	27	23	0	50	505
	7:00AM	12	106	13	131	15	29	0	44	9	9	0	18	193
	7:15AM	11	131	14	156	8	51	0	59	7	9	0	16	231
	7:30AM	22	159	33	214	27	35	0	62	10	15	0	25	301
	7:45AM	19	159	37	215	15	54	0	69	31	12	0	43	327
	Hourly Total	64	555	97	716	65	169	0	234	57	45	0	102	1052
	8:00AM	19	161	26	206	14	51	0	65	15	12	0	27	298
	8:15AM	16	139	23	178	16	45	0	61	22	14	0	36	275
	8:30AM	23	152	22	197	23	46	0	69	16	9	0	25	291
	8:45AM	17	154	19	190	11	55	0	66	12	19	0	31	287
	Hourly Total	75	606	90	771	64	197	0	261	65	54	0	119	1151
	9:00AM	21	147	19	187	12	39	0	51	20	18	0	38	276
	9:15AM	14	115	23	152	11	36	0	47	27	12	0	39	238
	9:30AM	16	141	28	185	18	20	0	38	17	9	0	26	249
	9:45AM	23	136	19	178	10	24	0	34	17	14	0	31	243
	Hourly Total	74	539	89	702	51	119	0	170	81	53	0	134	1006
	2:00PM	37	220	25	282	16	20	0	36	21	20	0	41	359
	2:15PM	46	192	29	267	17	19	0	36	34	22	0	56	359
	2:30PM	34	242	23	299	8	22	0	30	35	34	0	69	398
	2:45PM	33	210	23	266	18	37	0	55	38	29	0	67	388
	Hourly Total	150	864	100	1114	59	98	0	157	128	105	0	233	1504
	3:00PM	36	241	27	304	9	33	0	42	57	36	0	93	439
	3:15PM	28	228	30	286	14	24	0	38	59	30	0	89	413
	3:30PM	51	321	26	398	27	35	0	62	66	51	0	117	577
	3:45PM	44	273	32	349	15	31	0	46	57	50	0	107	502
	Hourly Total	159	1063	115	1337	65	123	0	188	239	167	0	406	1931
	4:00PM	37	335	31	403	17	22	0	39	62	56	0	118	560
	4:15PM	36	279	31	346	26	23	0	49	56	59	0	115	510
	4:30PM	32	287	25	344	30	25	0	55	73	56	0	129	528
	4:45PM	35	310	23	368	18	24	0	42	62	52	0	114	524
	Hourly Total	140	1211	110	1461	91	94	0	185	253	223	0	476	2122
	5:00PM	40	262	25	327	18	25	0	43	81	47	0	128	498
	5:15PM	43	235	32	310	18	18	0	36	72	34	0	106	452
	5:30PM	33	248	26	307	18	24	0	42	52	41	0	93	442
	5:45PM	22	191	28	241	8	29	0	37	42	31	0	73	351
	Hourly Total	138	936	111	1185	62	96	0	158	247	153	0	400	1743
	6:00PM	34	171	23	228	19	25	0	44	32	23	0	55	327
	6:15PM	28	170	29	227	11	14	0	25	22	15	0	37	289
	6:30PM	36	157	28	221	14	27	0	41	25	19	0	44	306
	6:45PM	27	166	17	210	14	25	0	39	16	21	0	37	286
	Hourly Total	125	664	97	886	58	91	0	149	95	78	0	173	1208
	Total	948	6661	856	8465	552	1112	0	1664	1192	901	0	2093	12222
	% Approach	11.2%	78.7%	10.1%	-	33.2%	66.8%	0%	_	57.0%	43.0%	0%	-	
	% Total	7.8%	54.5%	7.0%	69.3%	4.5%	9.1%	0%	13.6%	9.8%	7.4%	0%	17.1%	
	Lights and Motorcycles	883	6528	825	8236	546	1077	0	1623	1160	893	0	2053	11912
	Lights and Motorcycles	93.1%	98.0%	96.4%	97.3%	98.9%	96.9%	0%	97.5%	97.3%	99.1%	0%	98.1%	97.5%
,,,	Heavy	65	133	31	229	6	35	0	41	32	8	0	40	310
	% Heavy	6.9%	2.0%	3.6%	2.7%	1.1%	3.1%	0%	2.5%	2.7%	0.9%	0%	1.9%	2.5%

Tue Oct 22, 2024 Full Length (6 AM-10 AM, 2 PM-7 PM)

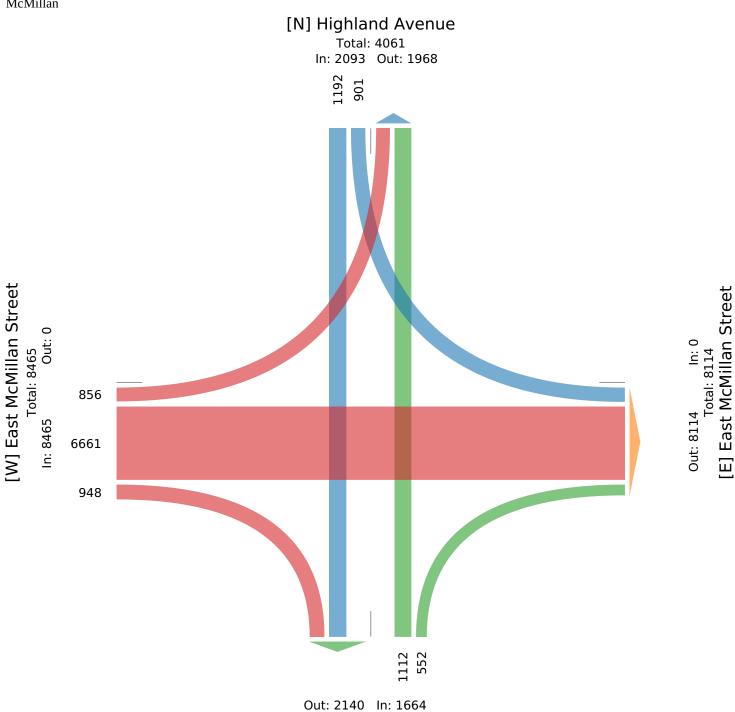
All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1239203, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan





Total: 3804 [S] Highland Avenue

Tue Oct 22, 2024

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1239203, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan



Leg	East McMil	lan Street			Highland A	venue			Highland Av	enue/			
Direction	Eastbound				Northbound				Southbound				
Time	R	T	L	App	R	T	U	App	T	L	U	Арр	Int
2024-10-22 7:30AM	22	159	33	214	27	35	0	62	10	15	0	25	301
7:45AM	19	159	37	215	15	54	0	69	31	12	0	43	327
8:00AM	19	161	26	206	14	51	0	65	15	12	0	27	298
8:15AM	16	139	23	178	16	45	0	61	22	14	0	36	275
Total	76	618	119	813	72	185	0	257	78	53	0	131	1201
% Approach	9.3%	76.0%	14.6%	-	28.0%	72.0%	0%	-	59.5%	40.5%	0%	-	-
% Total	6.3%	51.5%	9.9%	67.7%	6.0%	15.4%	0%	21.4%	6.5%	4.4%	0%	10.9%	-
PHF	0.864	0.960	0.804	0.945	0.667	0.856	-	0.931	0.629	0.883	-	0.762	0.918
Lights and Motorcycles	61	599	113	773	71	176	0	247	75	51	0	126	1146
% Lights and Motorcycles	80.3%	96.9%	95.0%	95.1%	98.6%	95.1%	0%	96.1%	96.2%	96.2%	0%	96.2%	95.4%
Heavy	15	19	6	40	1	9	0	10	3	2	0	5	55
% Heavy	19.7%	3.1%	5.0%	4.9%	1.4%	4.9%	0%	3.9%	3.8%	3.8%	0%	3.8%	4.6%

^{*}L: Left, R: Right, T: Thru, U: U-Turn

Tue Oct 22, 2024

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights and Motorcycles, Heavy)

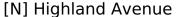
All Movements

ID: 1239203, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan

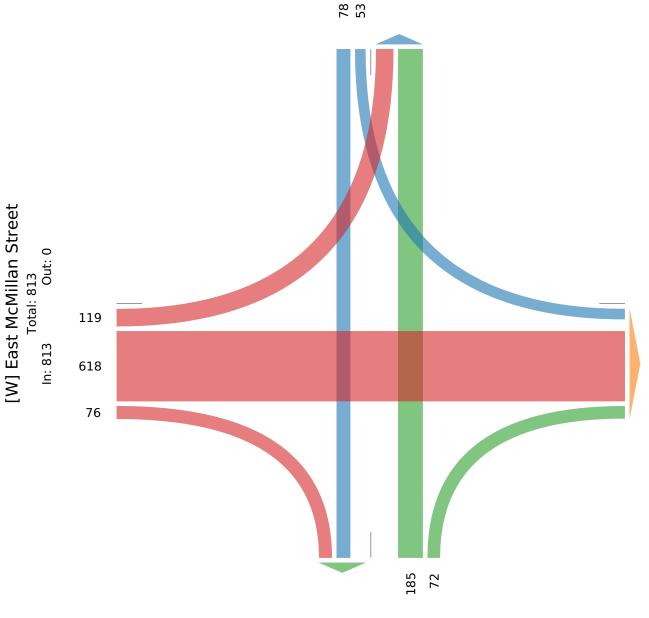


Provided by: SHA Engineering 6221 Thornberry Court, Mason, OH, 45040, US



Total: 435 In: 131 Out: 304





[E] East McMillan Street l3 In: 0 Total: 743

Out: 154 In: 257 Total: 411 [S] Highland Avenue

Tue Oct 22, 2024

PM Peak (3:30 PM - 4:30 PM) - Overall Peak Hour

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1239203, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan



Leg	East McMil	lan Street			Highland A	venue			Highland Av	enue/			
Direction	Eastbound				Northbound				Southbound				
Time	R	T	L	App	R	T	U	App	T	L	U	Арр	Int
2024-10-22 3:30PM	51	321	26	398	27	35	0	62	66	51	0	117	577
3:45PM	44	273	32	349	15	31	0	46	57	50	0	107	502
4:00PM	37	335	31	403	17	22	0	39	62	56	0	118	560
4:15PM	36	279	31	346	26	23	0	49	56	59	0	115	510
Total	168	1208	120	1496	85	111	0	196	241	216	0	457	2149
% Approach	11.2%	80.7%	8.0%	-	43.4%	56.6%	0%	-	52.7%	47.3%	0%	-	-
% Total	7.8%	56.2%	5.6%	69.6%	4.0%	5.2%	0%	9.1%	11.2%	10.1%	0%	21.3%	-
PHF	0.824	0.901	0.938	0.928	0.787	0.793	-	0.790	0.913	0.915	-	0.968	0.931
Lights and Motorcycles	158	1188	118	1464	84	106	0	190	237	214	0	451	2105
% Lights and Motorcycles	94.0%	98.3%	98.3%	97.9%	98.8%	95.5%	0%	96.9%	98.3%	99.1%	0%	98.7%	98.0%
Heavy	10	20	2	32	1	5	0	6	4	2	0	6	44
% Heavy	6.0%	1.7%	1.7%	2.1%	1.2%	4.5%	0%	3.1%	1.7%	0.9%	0%	1.3%	2.0%

^{*}L: Left, R: Right, T: Thru, U: U-Turn

Tue Oct 22, 2024

PM Peak (3:30 PM - 4:30 PM) - Overall Peak Hour

All Classes (Lights and Motorcycles, Heavy)

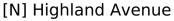
All Movements

ID: 1239203, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan



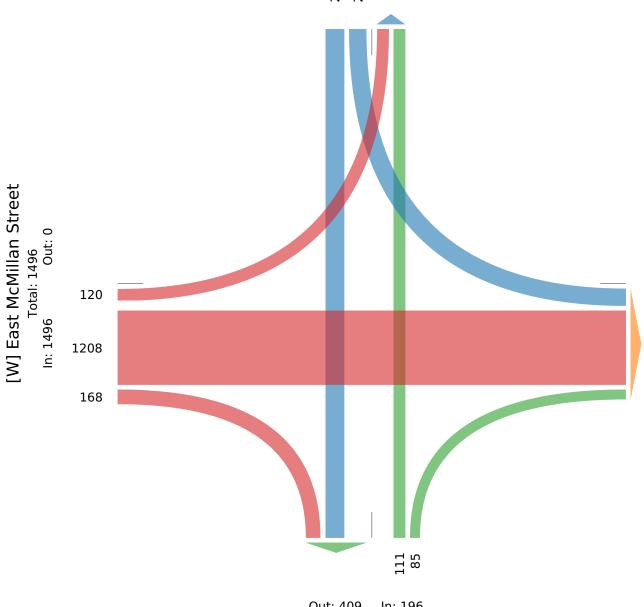
Provided by: SHA Engineering 6221 Thornberry Court, Mason, OH, 45040, US



Total: 688

In: 457 Out: 231





Out: 1509 In: 0 Total: 1509 [E] East McMillan Street

Out: 409 In: 196 Total: 605

[S] Highland Avenue

Tue Oct 22, 2024

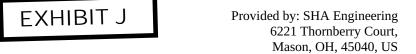
Full Length (10 AM-2 PM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

 $ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue \ at East$

McMillan



			venue	Highland Av			enue	Highland Av			lan Street	East McMill	Leg
				Southbound				Northbound				Eastbound	Direction
p Int	App	U	L	T	Арр	U	T	R	Арр	L	T	R	Time
3 21	23	0	10	13	31	0	16	15	163	16	131	16	2024-10-22 10:00AM
7 20	27	0	7	20	32	0	27	5	149	11	117	21	10:15AM
5 23	25	0	13	12	44	0	33	11	167	25	121	21	10:30AM
6 25	26	0	15	11	30	0	21	9	197	27	139	31	10:45AM
1 91	101	0	45	56	137	0	97	40	676	79	508	89	Hourly Total
8 27	38	0	20	18	36	0	20	16	205	20	156	29	11:00AM
8 26	38	0	23	15	35	0	22	13	194	21	144	29	11:15AM
2 28	32	0	16	16	27	0	14	13	228	22	176	30	11:30AM
4 27	34	0	19	15	40	0	29	11	203	27	149	27	11:45AM
2 111	142	0	78	64	138	0	85	53	830	90	625	115	Hourly Total
8 32	48	0	25	23	46	0	29	17	232	32	167	33	12:00PM
7 31	37	0	16	21	34	0	23	11	244	16	181	47	12:15PM
4 33	44	0	25	19	40	0	23	17	246	39	169	38	12:30PM
7 33	37	0	18	19	49	0	33	16	247	33	175	39	12:45PM
6 130	166	0	84	82	169	0	108	61	969	120	692	157	Hourly Total
6 30	46	0	18	28	42	0	31	11	213	24	162	27	1:00PM
9 34	49	0	29	20	50	0	34	16	244	29	192	23	1:15PM
3 32	43	0	23	20	46	0	31	15	233	24	181	28	1:30PM
2 29	32	0	17	15	29	0	16	13	229	24	174	31	1:45PM
0 125	170	0	87	83	167	0	112	55	919	101	709	109	Hourly Total
9 458	579	0	294	285	611	0	402	209	3394	390	2534	470	Total
-	-	0%	50.8%	49.2%	-	0%	65.8%	34.2%	-	11.5%	74.7%	13.8%	% Approach
6	12.6%	0%	6.4%	6.2%	13.3%	0%	8.8%	4.6%	74.0%	8.5%	55.3%	10.3%	% Total
9 439	559	0	291	268	589	0	384	205	3249	369	2466	414	Lights and Motorcycles
6 95.99	96.5%	0%	99.0%	94.0%	96.4%	0%	95.5%	98.1%	95.7%	94.6%	97.3%	88.1%	% Lights and Motorcycles
0 18	20	0	3	17	22	0	18	4	145	21	68	56	Heavy
6 4.19	3.5%	0%	1.0%	6.0%	3.6%	0%	4.5%	1.9%	4.3%	5.4%	2.7%	11.9%	% Heavy

^{*}L: Left, R: Right, T: Thru, U: U-Turn

Tue Oct 22, 2024

Full Length (10 AM-2 PM)

All Classes (Lights and Motorcycles, Heavy)

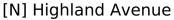
All Movements

ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan

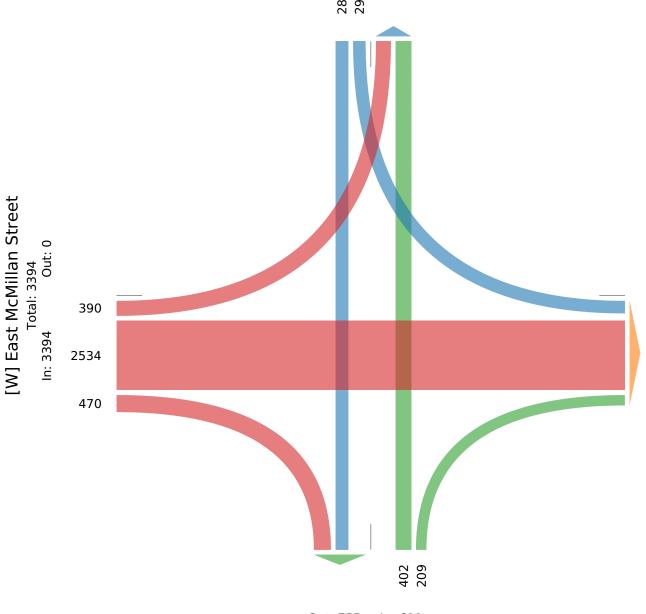


Provided by: SHA Engineering 6221 Thornberry Court, Mason, OH, 45040, US



Total: 1371 In: 579 Out: 792





[E] East McMillan Street 137 In: 0 Total: 3037 Out: 3037

Out: 755 In: 611 Total: 1366 [S] Highland Avenue

Tue Oct 22, 2024

AM Peak (10 AM - 11 AM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

 $ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue \ at East$

McMillan



Leg	East McMil	lan Street			Highland A	venue			Highland Av	venue			
Direction	Eastbound				Northbound				Southbound				
Time	R	T	L	App	R	T	U	App	T	L	U	Арр	Int
2024-10-22 10:00AM	16	131	16	163	15	16	0	31	13	10	0	23	217
10:15AM	21	117	11	149	5	27	0	32	20	7	0	27	208
10:30AM	21	121	25	167	11	33	0	44	12	13	0	25	236
10:45AM	31	139	27	197	9	21	0	30	11	15	0	26	253
Total	89	508	79	676	40	97	0	137	56	45	0	101	914
% Approach	13.2%	75.1%	11.7%	-	29.2%	70.8%	0%	-	55.4%	44.6%	0%	-	-
% Total	9.7%	55.6%	8.6%	74.0%	4.4%	10.6%	0%	15.0%	6.1%	4.9%	0%	11.1%	-
PHF	0.718	0.914	0.731	0.858	0.667	0.735	-	0.778	0.700	0.750	-	0.935	0.903
Lights and Motorcycles	72	493	73	638	40	91	0	131	54	45	0	99	868
% Lights and Motorcycles	80.9%	97.0%	92.4%	94.4%	100%	93.8%	0%	95.6%	96.4%	100%	0%	98.0%	95.0%
Heavy	17	15	6	38	0	6	0	6	2	0	0	2	46
% Heavy	19.1%	3.0%	7.6%	5.6%	0%	6.2%	0%	4.4%	3.6%	0%	0%	2.0%	5.0%

^{*}L: Left, R: Right, T: Thru, U: U-Turn

Tue Oct 22, 2024

AM Peak (10 AM - 11 AM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

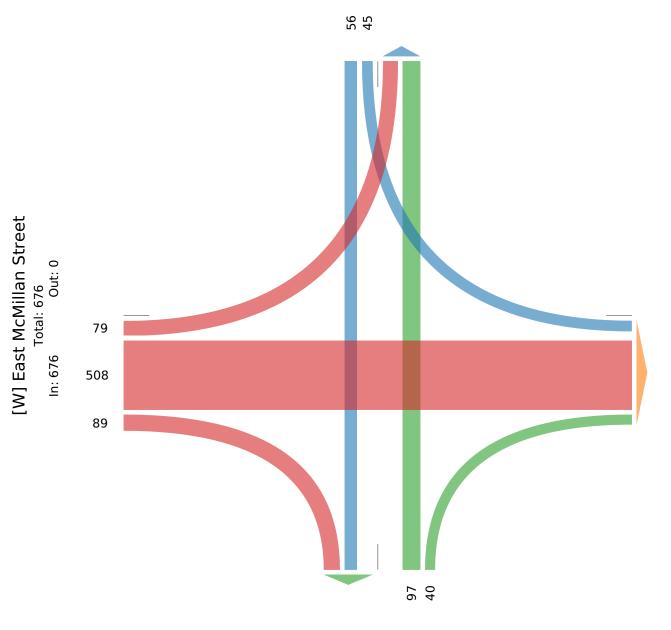
McMillan

EXHIBIT J

Provided by: SHA Engineering 6221 Thornberry Court, Mason, OH, 45040, US



Total: 277 In: 101 Out: 176



Total: 593

[E] East McMillan Street

Out: 145 In: 137 Total: 282 [S] Highland Avenue

Tue Oct 22, 2024

Midday Peak (12:30 PM - 1:30 PM) - Overall Peak Hour

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan	70, -04.5	03402, 3	nie Code	. mgm	anu Aven	ue at La	Sι						
Leg	East McMil	lan Street			Highland A	venue			Highland Av	enue			
Direction	Eastbound				Northbound				Southbound				
Time	R	T	L	Арр	R	T	U	App	T	L	U	App	Int
2024-10-22 12:30PM	38	169	39	246	17	23	0	40	19	25	0	44	330
12:45PM	39	175	33	247	16	33	0	49	19	18	0	37	333
1:00PM	27	162	24	213	11	31	0	42	28	18	0	46	301
1:15PM	23	192	29	244	16	34	0	50	20	29	0	49	343
Total	127	698	125	950	60	121	0	181	86	90	0	176	1307
% Approach	13.4%	73.5%	13.2%	-	33.1%	66.9%	0%	-	48.9%	51.1%	0%	-	-
% Total	9.7%	53.4%	9.6%	72.7%	4.6%	9.3%	0%	13.8%	6.6%	6.9%	0%	13.5%	-
PHF	0.814	0.909	0.801	0.962	0.882	0.890	-	0.905	0.768	0.776	-	0.898	0.953
Lights and Motorcycles	115	681	120	916	58	117	0	175	81	89	0	170	1261
% Lights and Motorcycles	90.6%	97.6%	96.0%	96.4%	96.7%	96.7%	0%	96.7%	94.2%	98.9%	0%	96.6%	96.5%

EXHIBIT J

4 0

0%

3.3%

6

3.3%

5

5.8%

Heavy

% Heavy

12

9.4%

17

2.4%

5

4.0%

34

3.3%

3.6%

Provided by: SHA Engineering 6221 Thornberry Court, Mason, OH, 45040, US

0

3.4%

0%

1.1%

46

3.5%

^{*}L: Left, R: Right, T: Thru, U: U-Turn

Tue Oct 22, 2024

Midday Peak (12:30 PM - 1:30 PM) - Overall Peak Hour

All Classes (Lights and Motorcycles, Heavy)

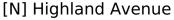
All Movements

 $ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue \ at East$

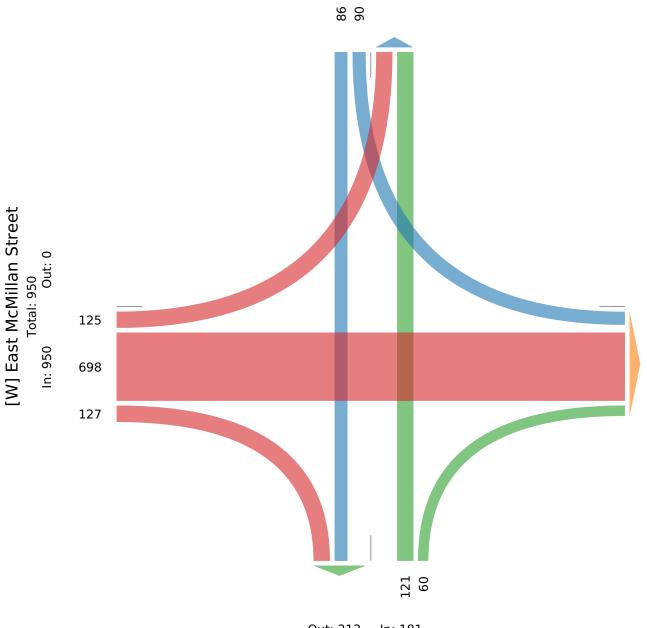
McMillan



EXHIBIT J



Total: 422 In: 176 Out: 246



Out: 848 In: 0 Total: 848 [E] East McMillan Street

Provided by: SHA Engineering 6221 Thornberry Court,

Mason, OH, 45040, US

Out: 213 In: 181 Total: 394 [S] Highland Avenue

Tue Oct 22, 2024

PM Peak (1 PM - 2 PM)

All Classes (Lights and Motorcycles, Heavy)

All Movements

ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue at East

McMillan



Leg	East McMil	lan Street			Highland A	venue			Highland Av	venue			
Direction	Eastbound				Northbound				Southbound				
Time	R	T	L	App	R	T	U	App	T	L	U	Арр	Int
2024-10-22 1:00PM	27	162	24	213	11	31	0	42	28	18	0	46	301
1:15PM	23	192	29	244	16	34	0	50	20	29	0	49	343
1:30PM	28	181	24	233	15	31	0	46	20	23	0	43	322
1:45PM	31	174	24	229	13	16	0	29	15	17	0	32	290
Total	109	709	101	919	55	112	0	167	83	87	0	170	1256
% Approach	11.9%	77.1%	11.0%	-	32.9%	67.1%	0%	-	48.8%	51.2%	0%	-	-
% Total	8.7%	56.4%	8.0%	73.2%	4.4%	8.9%	0%	13.3%	6.6%	6.9%	0%	13.5%	-
PHF	0.879	0.923	0.871	0.942	0.859	0.824	-	0.835	0.741	0.750	-	0.867	0.915
Lights and Motorcycles	96	688	97	881	53	108	0	161	79	86	0	165	1207
% Lights and Motorcycles	88.1%	97.0%	96.0%	95.9%	96.4%	96.4%	0%	96.4%	95.2%	98.9%	0%	97.1%	96.1%
Heavy	13	21	4	38	2	4	0	6	4	1	0	5	49
% Heavy	11.9%	3.0%	4.0%	4.1%	3.6%	3.6%	0%	3.6%	4.8%	1.1%	0%	2.9%	3.9%

^{*}L: Left, R: Right, T: Thru, U: U-Turn

Tue Oct 22, 2024

PM Peak (1 PM - 2 PM)

All Classes (Lights and Motorcycles, Heavy)

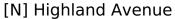
All Movements

 $ID: 1248004, Location: 39.126778, -84.503402, Site Code: Highland Avenue \ at East$

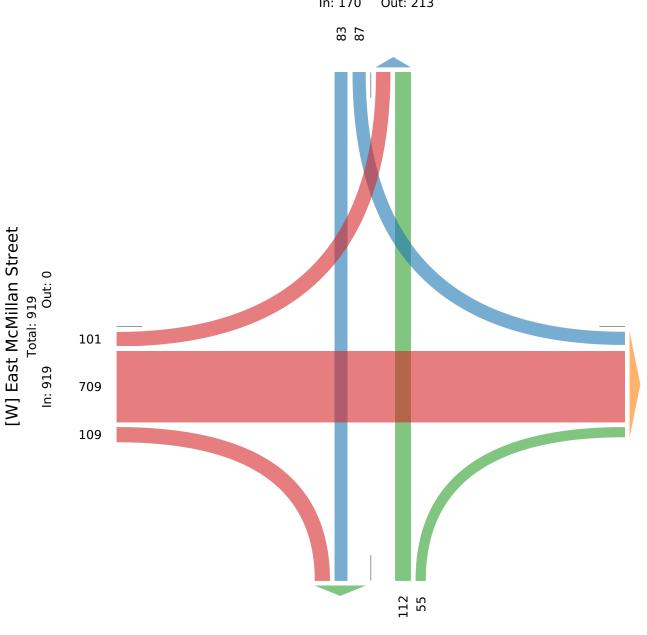
McMillan



Provided by: SHA Engineering 6221 Thornberry Court, Mason, OH, 45040, US



Total: 383 In: 170 Out: 213



Out: 851 In: 0 Total: 851 [E] East McMillan Street

Out: 192 In: 167 Total: 359 [S] Highland Avenue

Appendix D Turn Lane Warrants

4-Lane Highway Left Turn Lane Warrant

Study Year: 2025 STREET TYPE = UNDIVIDED Chick-fil-A, Highland Avenue (Build-PM = WARRANTED AM = WARRANTED East McMillan Street at Drive A 70 Divided 60 Left Turn Lane Required 50 40 36. 30 Undivided **PM** 20 10 Left Turn Lane Not Required 0 0 200 400 600 800 1000 2000 1200 1400 1600 1800



Left Turn Volume (dhv)

Ohio Department of Transportation State Highway Access Management Manual **Opposing Traffic (dhv)**

Issued December 2001 Version 3-12-03 Page 41



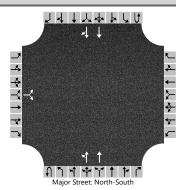
Highland Avenue at Drive A

REQ	Intersection	Advancing	Right Turning	Result
		Traffic Volume		
1	Southbound RT - 2025 AM Build	228	67	NO
2	Southbound RT - 2025 PM Build	446	54	NO
3				
4				
5				
6				
7				
8				
9				
10				
11				
12				
13				
14				
15				
16				
17				
18				
19				
20				

Appendix E Capacity Analysis

HCS Two-Way Stop-Control Report **EXHIBIT** J Seneral Information **Site Information** JΑ Highland Avenue at Drive A Analyst Intersection Agency/Co. SHA Engineering Jurisdiction Cincinnati Date Performed 11/18/2024 East/West Street Drive A 2025 Analysis Year North/South Street Highland Avenue 0.92 Time Analyzed Midday Peak - Build Peak Hour Factor Intersection Orientation North-South Analysis Time Period (hrs) 0.25 **Project Description** Chick-fil-A, Highland Avenue

Lanes

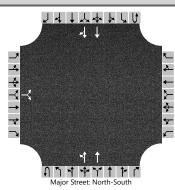


		_														
Vehicle Volumes and Adj	justme	nts														
Approach		Eastb	ound			Westl	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	T	R	U	L	T	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
Configuration			LR							LT	Т				Т	TR
Volume (veh/h)		44		34						36	260				161	67
Percent Heavy Vehicles (%)		3		3						3						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.5		6.9						4.1						
Critical Headway (sec)		6.86		6.96						4.16						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						
Follow-Up Headway (sec)		3.53		3.33						2.23						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	Т		85							39						
Capacity, c (veh/h)			648							1308						
v/c Ratio			0.13							0.03						
95% Queue Length, Q ₉₅ (veh)			0.4							0.1						
Control Delay (s/veh)			11.4							7.8	0.2					
Level of Service (LOS)			В							А	А					
Approach Delay (s/veh)		1	1.4							1	.1					
Approach LOS			 В								Α					

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HCS Two-Way Stop-Control Report **EXHIBIT J** Seneral Information **Site Information** JΑ Highland Avenue at Drive A Analyst Intersection Agency/Co. SHA Engineering Jurisdiction Cincinnati Date Performed 11/18/2024 East/West Street Drive A 2025 Analysis Year North/South Street Highland Avenue PM Peak - Build 0.92 Time Analyzed Peak Hour Factor Intersection Orientation North-South Analysis Time Period (hrs) 0.25 **Project Description** Chick-fil-A, Highland Avenue

Lanes

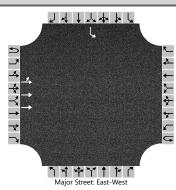


Vehicle Volumes and Ad	iustme	nts														
Approach			oound			West	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6
Number of Lanes		0	1	0		0	0	0	0	0	2	0	0	0	2	0
		0	LR	0		0	0	0	0	LT	T	0	0	0	T	TR
Configuration		20	LK	20												-
Volume (veh/h)	-	29		29		_				23	246				442	54
Percent Heavy Vehicles (%)	-	3		3						3						
Proportion Time Blocked																
Percent Grade (%)			0													
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up H	eadwa	ys														
Base Critical Headway (sec)		7.5		6.9						4.1						П
Critical Headway (sec)		6.86		6.96						4.16						
Base Follow-Up Headway (sec)		3.5		3.3						2.2						П
Follow-Up Headway (sec)		3.53		3.33						2.23						
Delay, Queue Length, an	d Leve	l of S	ervice													
Flow Rate, v (veh/h)	Т		63							25		П				Т
Capacity, c (veh/h)			486							1018						
v/c Ratio			0.13							0.02						
95% Queue Length, Q ₉₅ (veh)			0.4							0.1						
Control Delay (s/veh)			13.5							8.6	0.2					П
Level of Service (LOS)			В							А	А					
Approach Delay (s/veh)		1:	3.5							0	.9					
Approach LOS			В							,	Α					

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EXHIBIT J	HCS Two-Way Stop	-Control Report	
General information		Site Information	
Analyst	JA	Intersection	E. McMillan Street at Drive B
Agency/Co.	SHA Engineering	Jurisdiction	Cincinnati
Date Performed	11/18/2024	East/West Street	East McMillan Street
Analysis Year	2025	North/South Street	Drive B
Time Analyzed	Midday Peak - Build	Peak Hour Factor	0.92
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	Chick-fil-A, Highland Avenue		

Lanes



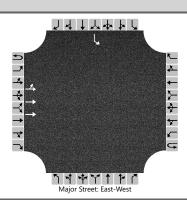
Vehicle Volumes and Ad	ustme	nts															
Approach		Eastb	ound			Westl	bound			North	bound			Southbound			
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	Т	R	
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Number of Lanes	0	0	3	0	0	0	0	0		0	0	0		1	0	0	
Configuration		LT	Т											L			
Volume (veh/h)		102	879											119			
Percent Heavy Vehicles (%)		3												3			
Proportion Time Blocked																	
Percent Grade (%)															0		
Right Turn Channelized																	
Median Type Storage				Undi	vided												
Critical and Follow-up H	eadwa	ys															
Base Critical Headway (sec)		5.3												6.4			
Critical Headway (sec)		5.36												5.76			
Base Follow-Up Headway (sec)		3.1												3.8			
Follow-Up Headway (sec)		3.13												3.83			
Delay, Queue Length, an	d Leve	l of S	ervice														
Flow Rate, v (veh/h)		111												129			
Capacity, c (veh/h)		1150												428			
v/c Ratio		0.10												0.30			
95% Queue Length, Q ₉₅ (veh)		0.3												1.3			
Control Delay (s/veh)		8.5	0.7											17.0			
Level of Service (LOS)		А	А		Ì		Ì			Ì				С			
Approach Delay (s/veh)		1	.5					•						1	7.0		
Approach LOS		,	Ą											(С		

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HCS Two-Way Stop-Control Report

General Information		Site Information	
Analyst	JA	Intersection	E. McMillan Street at Drive B
Agency/Co.	SHA Engineering	Jurisdiction	Cincinnati
Date Performed	11/18/2024	East/West Street	East McMillan Street
Analysis Year	2025	North/South Street	Drive B
Time Analyzed	PM Peak - Build	Peak Hour Factor	0.92
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	Chick-fil-A, Highland Avenue		

Lanes



Approach		Eastb	ound			West	oound			North	bound			South	bound	
Movement	U	L	Т	R	U	L	Т	R	U	L	Т	R	U	L	T	R
Priority	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Number of Lanes	0	0	3	0	0	0	0	0		0	0	0		1	0	0
Configuration		LT	Т											L		
Volume (veh/h)		77	1443											89		
Percent Heavy Vehicles (%)		3												3		
Proportion Time Blocked																
Percent Grade (%)														()	
Right Turn Channelized																
Median Type Storage				Undi	vided											
Critical and Follow-up He	adwa	ys														
Base Critical Headway (sec)		5.3												6.4		
Critical Headway (sec)		5.36												5.76		
Base Follow-Up Headway (sec)		3.1												3.8		
Follow-Up Headway (sec)		3.13												3.83		
Delay, Queue Length, and	l Leve	l of Se	ervice													
Flow Rate, v (veh/h)		84												97		
Capacity, c (veh/h)		1150												351		
v/c Ratio		0.07												0.28		
95% Queue Length, Q ₉₅ (veh)		0.2												1.1		
Control Delay (s/veh)		8.4	0.7											19.1		

1.1

Α

Level of Service (LOS)

Approach Delay (s/veh)

Approach LOS

Vehicle Volumes and Adjustments

19.1

C

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HCS Signalized Intersection Results Summary

General Information									Intersec	tion In	\ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \					
Agency SHA Engineering									Duration	, h	0.250		# 5			
Analyst		JA	Analys	sis Date	11/16/	2024		Area Typ	e	Other	•	<u></u>		<u> </u>		
Jurisdiction		Cincinnati	Time F		Miday		PHF		0.92		→	w∳E	₽ -			
						2025		_	Analysis	Period		00	<u>-</u> 4 ¥		*	
Intersection		Highland Avenue at	Driv	File Na										+ 2	<u> </u>	
Project Descrip	tion	Chick-fil-A, Cincinna		1 110 110	File Name 1-3-Midday-2025-Build-Highland-McMillan.xus									1 1 1 1 1 1 1	†* (*	
		omen m 7 , omen m														
Demand Infor					EB			WI	3		NB			SB		
Approach Move				L	Т	R	L	Т	R	L	T	R	<u> </u>	Т	R	
Demand (v), v	/eh/h			154	708	136					142	60	99	96		
Signal Informa	tion					1 15		7								
	ır	Reference Phase	2	1	177	1	Lą.						1×		Z.	
Cycle, s	60.0		End	ł		l ti	" 3					1	2	3	$\overline{\Delta}$	
Offset, s		Reference Point		Green		24.0	13.0	0.0		0.0						
Uncoordinated Force Mode	No	Simult. Gap E/W Simult. Gap N/S	On	Yellow	-	4.0 2.0	4.0 2.0	0.0		0.0				_	Į.	
Force Mode	Fixed	Simuit. Gap N/S	On	Red	2.0	2.0	2.0	0.0	0.0	0.0		5	6	1	0	
Timer Results				EBI		EBT	WBI		WBT	NE	RI	NBT	SBI		SBT	
Assigned Phas						4	1,12,	_	,,,,,	142		2	1		6	
Case Number						12.0						7.3	1.0		4.0	
Phase Duration) S				_	19.0						30.0	11.0	_	41.0	
Change Period	<u> </u>	·) s				6.0						6.0		_	6.0	
Max Allow Hea		·				3.1						0.0	6.0 3.1	_	0.0	
Queue Clearan						10.8						0.0	3.8	_	0.0	
Green Extension		, = ,				2.2						0.0	0.1		0.0	
Phase Call Pro		(g °), °				1.00								3	0.0	
Max Out Proba						0.03		_					0.00	_		
Movement Gro	oup Res	ults			EB	W					NB			SB		
Approach Move				L	T	R	L	T	R	L	T	R	L	T	R	
Assigned Move				7	4	14					2	12	1	6		
Adjusted Flow		,		292	543	250					154	65	108	104		
		ow Rate (s), veh/h/l	n	1847	1900	1717					1900	1610	1810	1900		
Queue Service				8.8	7.8	8.0					3.2	1.5	1.8	1.5		
Cycle Queue C		e Time (<i>g շ</i>), s		8.8	7.8	8.0				_	3.2	1.5	1.8	1.5		
Green Ratio (g				0.22	0.22	0.22	\perp		+		0.40	0.40	0.52	0.58		
Capacity (c), v				400	823	372			-	_	760	644	705	1109		
Volume-to-Cap				0.729	0.660				-		0.203	0.101	0.153	0.094		
	<u> </u>	/In (95 th percentile		160.1	143.6	135					57.6	23.6	25.5	21.4		
		eh/ln (95 th percenti		6.4	5.7	5.4					2.3	0.9	1.0	0.9		
		RQ) (95 th percent	ile)	0.42	0.38	0.36					0.19	0.24	0.28	0.09		
Uniform Delay	` ,			21.9	21.5	21.6					11.8	11.3	7.7	5.5		
Incremental Delay (d 2), s/veh			1.0	0.3	0.8					0.6	0.3	0.0	0.2			
Initial Queue Delay (d 3), s/veh			0.0 22.8	0.0	0.0					0.0	0.0	0.0	0.0			
Control Delay (d), s/veh					21.8	22.3					12.4	11.6	7.7	5.7		
Level of Service (LOS)				C	С	С				10	B	В	A 0.7	Α		
Approach Delay, s/veh / LOS			22.2	<u>′</u>	С	0.0			12.	7	В	6.7		Α		
Intersection De	iay, s/ve	en / LOS				18	3.6						В			
Multimodal Re	sults				EB			WB			NB			SB		
Pedestrian LOS		/LOS		1.93		В	1.93		В	2.0		В	2.05		В	
				0.94	_	A	1.00			0.8		A	0.84		A	
Bicycle LOS Score / LOS						, ,				0.0	J	,,	0.05		, ,	

HCS Signalized Intersection Results Summary

Conoral Inform	action						1	Into	rooti	on Inf	Į į	4 74	اعداد					
General Information									Intersection Information Duration, h 0.250						Į Ļ			
Agency		SHA Engineering							· ·						K.			
Analyst		JA	-					Area Type			Other		<u>^</u>	w∱E	<u>-</u>			
Jurisdiction Cincinnati				Time F		PM Build PHF 2025 - Build Analysis					<u> </u>	0.92			W T = 8	-		
Urban Street Highland Avenue					sis Year			<u> </u>		-	Period	1> 7:0)()			¥		
Intersection		Highland Avenue at		File Na	File Name 1-7-PM-2025-Build-Highland-McMillan.xus									-	<u>† </u>			
Project Descrip	tion	Chick-fil-A, Cincinna	atı	_	_	_	_	_	_	_	_	_	_		4 1 4 7	r II		
Demand Information					EB		1	W	'R			NB			SB			
Approach Move				L	T	R	L	T 7		R		T	R	L	T	R		
Demand (v), v				142	1215		1			- ` -		127	92	223	248	1		
Bomana (v); v	G11/11				1210	170						121	02	220	210			
Signal Informa	ation					144		\top										
Cycle, s	60.0	Reference Phase	2	1	'-	1	, 					\	L	1	-	~		
Offset, s	0	Reference Point	End	Green	7.6	15.8	18.7	0.0	1	0.0	0.0		1	2	3	4		
Uncoordinated	No	Simult. Gap E/W	On	Yellow		4.0	4.0	0.0		0.0	0.0	_						
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.0	2.0	2.0	0.0		0.0	0.0		5	6	7	8		
Timer Results				EBI	L L	EBT	WBI	-	WB	BT	NBI	-	NBT	SBI	-	SBT		
Assigned Phase	е					4							2	1		6		
Case Number						12.0							7.3	1.0		4.0		
Phase Duration	1, S					24.7							21.8	13.6	6	35.3		
Change Period	, (Y+R	c), S				6.0							6.0	6.0		6.0		
Max Allow Hea	dway(<i>N</i>	<i>ИАН</i>), s				3.1	.1						0.0			0.0		
Queue Clearan	ce Time	e (g s), s				15.1							7		7.4			
Green Extension	n Time	(g e), s				3.6							0.0 0.3			0.0		
Phase Call Pro	bability					1.00							0.98	3				
Max Out Proba	bility					0.14								0.00)			
Mayamant Cra	un Bee	vulto.			ГΡ			١٨/٦	,			NID			CD			
Movement Gro		suits		-	EB			WE				NB	n		SB	В		
Approach Move				7	T	14	L	Т		R	L	T 2	R 12	1	Т 6	R		
_		\ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \		450	833	383	-		+	-		138	100	242	270			
Adjusted Flow I		<i>'</i>	n	1868	1900	1744	\vdash		+	\dashv								
Queue Service		ow Rate (s), veh/h/l	n .	13.1	11.6	11.6			+	-		1900 3.5	1610 2.9	1810 5.4	1900 5.1			
Cycle Queue C		• /:		13.1	11.6	11.6			+	-		3.5	2.9	5.4	5.1			
Green Ratio (g		e Tillie (<i>g c)</i> , s		0.31	0.31	0.31			+	-		0.26	0.26	0.42	0.49			
Capacity (c), v				581	1183	543			+	-		500	423	608	929			
Volume-to-Cap		tio (V)		0.774	_				+	-		0.276	0.236	0.398	0.290			
		√In (95 th percentile	١	221.7	199.9				+	-		70	50.9	83	85.7			
		eh/In(95 th percenti		8.9	8.0	7.6				-		2.8	2.0	3.3	3.4			
		RQ) (95 th percent	-	0.58	0.53	0.50				\dashv		0.23	0.51	0.92	0.34			
Uniform Delay		, , , , , , , , , , , , , , , , , , ,		18.7	18.2	18.2						17.6	17.4	11.9	9.1			
Incremental De	. ,			1.2	0.3	0.6				-		1.4	1.3	0.2	0.8			
Initial Queue De	- \	*		0.0	0.0	0.0						0.0	0.0	0.2	0.0			
	Control Delay (d), s/veh			19.9	18.5	18.9				-		18.9	18.7	12.0	9.9			
Level of Service (LOS)				B	B	B						В	В	B	A			
				19.0		В	0.0			\rightarrow	18.8		В	10.9		В		
	Approach Delay, s/veh / LOS Intersection Delay, s/veh / LOS			13.0			7.3				10.0			B				
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Multimodal Re	sults				EB			WE	3			NB			SB			
Pedestrian LOS		/ LOS		1.93		В	1.93	-	В		2.10		В	2.07	-	В		
Bicycle LOS So				1.17		A				\neg	0.88	_	A	1.33		A		
bicycle 203 3cole / 203											,,,,							



February 21, 2025



Mr. Jamal A. Adhami, PE, PTOE SHA Engineering LLC 6221 Thornberry Court Mason, OH 45040

Dear Mr. Adami:

The Department of Transportation and Engineering (DOTE) has completed the review of the Traffic Analysis Report for Proposed Chick-Fil-A Highland Ave, Cincinnati, Ohio (TIS) updated on February 20, 2025.

DOTE approves the findings and recommendations of the TIS as stated in the report.

Any DOTE comments or requirements during the plan review or permitting process will supersede the approval of the TIS.

Should you have any questions or concerns, please contact Bryan Williams at bryan.williams@cincinnati-oh.gov or 352-3462.

Sincerely,

Bryan Williams, P.E. Division Manager



oh.gov>

Subject: [External Email] CFA Updated plans

External Email Communication

Stacey, Emily

I hope you are both hunkered down today In advance of our call tomorrow morning, I wanted to update you on the progress on the past few months as well as send the updated plans (attached).

CFA studied the redesign as requested from our meeting to move the building to the corner and have the drivethrough adjacent to the building (not around it). This did not work for a couple of reasons. Moving it to the north of the building along Highland would eliminate the entrance off of Highland avenue because the drive through would be extending all the way to the location of the (former) entrance on Highland. Cars would not be able to enter the active drive through there. That drive on Highland is essential for the traffic flow of the site, but more importantly to us, is necessary for access to the apartment building loading dock/trash compactor. We intend to keep an easement over the drive from Highland for that purpose. Locating the drive adjacent to the building to the west did not work because of the west property line as well as the large retaining wall that goes through the site. This is easier to show in street view which we can do tomorrow.

We also learned from CFA that their business has changed dramatically since covid and that they are closing locations that they opened just a few years ago because of design flow on the site/site size cannot accommodate the amount of traffic they drive. Regarding traffic, they did the study per DOTE's request (attached) which I believe recommends a dedicated left turn on Highland going north. DOTE is not concerned with traffic here which is good.

They were able to make changes that can accomplish the goal of bringing a presence to the street in a few ways:

- 1. Increased the landscape buffer. On McMillan, there is a 5' sidewalk, 5' tree lawn (per DOTE requirements) and then a planted landscape buffer. On Highland, there is a 7' sidewalk and then a landscape buffer. The landscaping is shown on the landscape plan.
- 2. Privacy screening, masonry walls and fencing on McMillan. On McMillan where the order point is, they are proposing adding a privacy screening canopy built of brick with aluminum storefront infill panels. This is shown on the elevations as "order point elevation" and "perspective view" and it is also outlined in light-grey dashed line on the site plan. It is a substantial structure designed to bring the building to the street while screening the cars.
- 3. Walls/fencing on Highland behind the landscape buffer there will be a masonry wall with a 3.5' fence on top. You can see the outline of this on the siteplan and landscape plan and a sample of what it will look like coming off of the privacy screen on "order point elevation" and "perspective view"

Since the site is 1.5 acres we are hoping to pursue a Planned Development here. I'm looking forward to reviewing this with you tomorrow but if any questions come up in the meantime, please let me know.

Stay warm! Patrice

Patrice Eby Burke <image001.png> Vice President



Project: Chick Fil A - 198 E McMillan Street Community Engagement

Presented to:

Mt Auburn Community Development Corporation – July, 2024

Mt Auburn Community Council - August 19, 2024

Mt Auburn Community Council - October 21, 2024 (minutes attached)

Corryville Community Development Corporation - November, 2024

University of Cincinnati & Port of Greater Cincinnati – September, 2024

Mt Auburn Community Council Board Site Visit – February 7, 2025

Future Meetings:

Mt Auburn Community Council – February 18, 2025



Gibbs, Kyle

From: John Wulsin <johnw@plattedesign.com>
Sent: Tuesday, March 18, 2025 11:34 AM

To: Gibbs, Kyle

Subject: [External Email] Proposed Zone Change to Planned Development (PD) at William

Howard Taft, Highland, and McMillian in Mt. Auburn

Follow Up Flag: Follow up Flag Status: Flagged

You don't often get email from johnw@plattedesign.com. Learn why this is important

External Email Communication

I'm writing in regards to the <u>proposed zone change</u> at the corner of McMillan and Highland to build a two-lane drive-thru Chick-fil-A.

The proposed single-story, double-laned drive-thru is inappropriate for this location because it further entrenches this location as a car-first and car-only destination. If built as proposed, the line of idling cars will send a loud, dirty signal to would-be pedestrians that this is not a place to be walking, this is a place only for cars.

Our zoning rules should be leveraged to **create a better build environment**, or at the very least do no harm and *do not make things worse*. This proposed two-lane drive thru would commit a high-profile corner lot to being a carcentric design for the next generation (or more).

The Planning Commission should be supporting projects that facilitate walking and cycling as viable alternatives to driving. Connecting Mt Auburn to Corryville and the broader UC campus is a vital, long-term project and any infill development along McMillan presents a once-in-a-generation opportunity to right the wrongs of past, carcentric development patterns.

There is housing already on the NW part of this parcel, and this proposed lot split indicates that the owners don't need all that surface parking for the existing housing. The current surface parking lot on the SE corner is not a good use of that land, so I'm fully in support of infill development that is a higher and better use. Personally, I would not mind a Chick-fil-A at this location **if it were part of a larger, mixed-use building at the corner**. As a point of comparison, when Uptown Rents developed the mixed-used building at 2899 Short Vine, they cleverly designed a Fifth Third bank that faces the corner and still has 3 drive-thru lanes. That project has apartments above and demonstrates that a thoughtful design can incorporate a drive-thru while still appropriately facing the street. I would encourage the Commission to deny this proposal and challenge the owners to develop a new plan that better addresses the street and creates an attractive, safe streetscape.

Related to this site, I would encourage the Planning Commission to proactively work with DOTE to plan for a healthy, walkable neighborhood. We need to prioritize traffic calming to improve the pedestrian experience along McMillan and Taft: two-way traffic, narrower lanes, street trees and curb bumpouts. When we make it safer and more attractive for pedestrians and cyclists, we create a future that will be more sustainable and support higher density, walkable developments.

Thank you for your consideration, -John

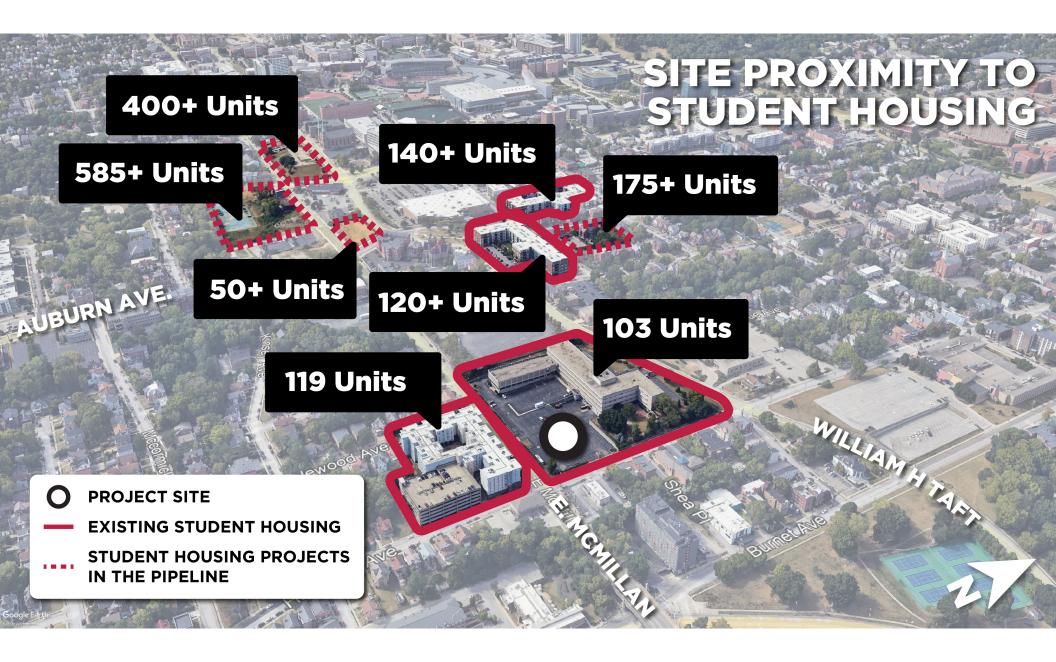


EXHIBIT P

Date: January 24, 2024

Description: Rezone to PD

6.6399 Acres

Location: City of Cincinnati,

Hamilton County, Ohio



Situated in Section 14, Town 3, Fractional Range 2 Between the Miamis, The City of Cincinnati, Hamilton County, Ohio and being 6.6399 acres to be re-zoned to Planned Development (PD) further described as follows:

Begin at the intersection of the centerline of East McMillan Street and Highland Avenue, said intersection being the True Point of Beginning;

thence, departing said Highland Avenue and with said East McMillan Street, North 83° 55' 20" West, 525.97 feet:

thence, departing said East McMillian Street, North 05° 48' 41" East, 451.77 feet to the centerline of William Howard Taft Road;

thence, with said William Howard Taft Road, North 75° 34' 52" East, 560.55 feet to the intersection of the centerline of said William Howard Taft and Highland Avenue;

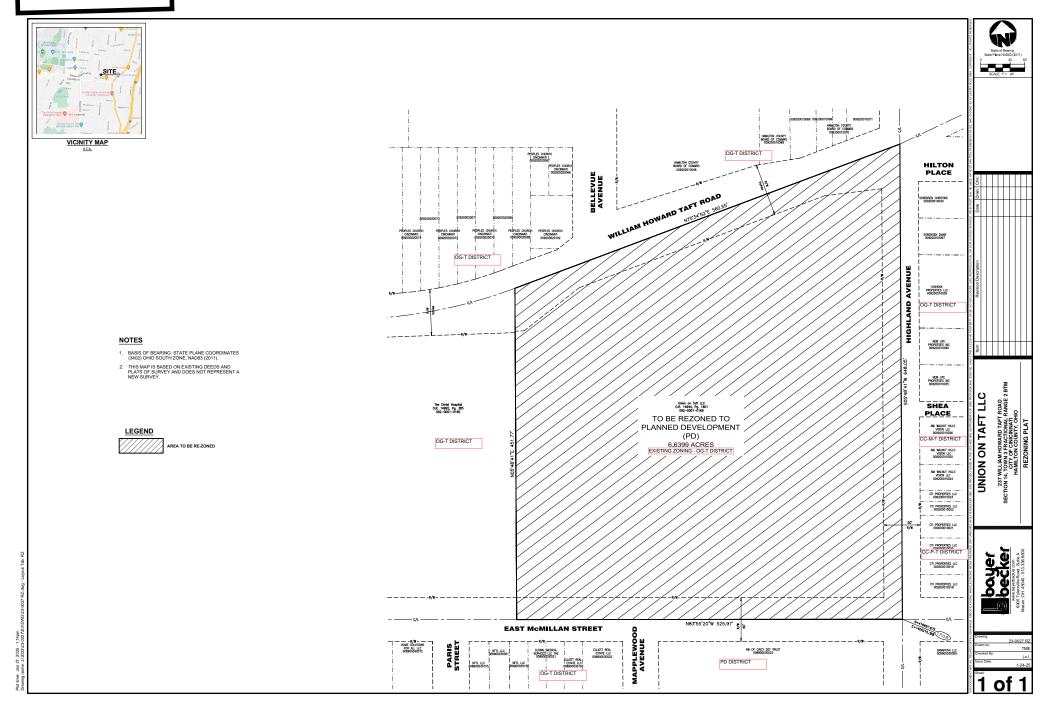
thence, departing said William Howard Taft Road and with said Highland Avenue South 05° 48' 41" West, 648.05 feet to the True Point of Beginning containing 6.6399 acres.

Basis of Bearings: NAD83(2011) Ohio State Plane Coordinates, South Zone (3402).

The above description is a complete, proper and legal description of the property by deeds and plats of record.

Jeffrey O. Lambert Registered Surveyor #7568 in the State of Ohio

EXHIBIT Q





March 31, 2025

City of Cincinnati Planning Commission

Dear Commissioners,

Thank you for your consideration of the approval of the Planned Development at McMillan & Highland Avenues consisting of a residential housing development and a proposed Chick Fil A drive through. The report from the Department of City Planning outlines the specifics of the request and the report from McBride Dale Clarion explains why a PD is appropriate in this location. As a developer and manager of thousands of apartments as well as commercial space since the mid-1980s we are the most experienced residential operator in this market, and we are confident that a Chick Fil A drive through is a superior use at this site. We offer the additional information for consideration:

Land Use and Context

The site has been a surface parking lot since the 1960s, and there is ample parking for the residential building, Union on Taft. As such, we considered alternative uses to surface parking, including housing, which is our expertise, and we do not think this is an optimal site for more apartments. The market has demonstrated that this is not a successful spot for housing, as the existing development across the street has struggled to maintain occupancy since it was built fifteen years ago. There are many developments closer to campus currently under construction or ready to begin construction that will provide the necessary supply for the next several years. These sites are superior for housing because of their location more proximate to campus and in already established housing zones. Union on Taft, although a part of this site, is also a superior location because of how it is sited on William H Taft at a higher elevation and away from the intersection of E McMillan & Highland. Other uses that would have street retail are also not desired, as the demand for small retail cannot support smaller shops in this location. Last, the site is directly across the street from a Taco Bell drive-through, a convenient store and several blocks from a major interstate highway. It is located in the second largest employment center in the City which also serves thousands of visitors every day at the University and the hospitals. The need for another fast-serve restaurant option along a major automobile thoroughfare became clear, with Chick Fil A as the optimal operator.

Operator and Job Creation

Chick Fil A is known to be an operator with outstanding service and a focus on giving back to the community. They anticipate hiring 80-100 associates at this location in addition to management. Chick Fil A is known to be an attractive employer because of their pay and benefits, including tuition reimbursement. They are focused on giving back to the community and donate thousands of dollars every year per restaurant to local charities. They keep their



facilities clean inside and out, continue to innovate, and strive for excellence in their products and service.

Community Engagement

The letter enclosed from the Mt Auburn Community Council details the engagement and outreach that we have conducted over the past six months. The MACC questions center around traffic, and the concern that a Chick Fil A could increase congestion. In fact Chick Fil A studied moving the building to the corner as suggested by Planning, and the resulting design would eliminate an entrance/exit and be detrimental to operations, namely by prolonging order times which could potentially cause congestion and only exacerbate a concern of the community. It is clear that the community is not concerned that the building is not at the corner or that there is a drive through use. DOTE concurs with the traffic study which concludes that traffic can be handled onsite and the development would not require modifications to the right-of-way.

Incentives

The proposed Planned Development consists of over \$30MM of improvements done privately and completely without economic incentive. Union on Taft was an underutilized office building that was almost entirely vacant after being on the market for lease for over a year. Undertaking an adaptive reuse of the structure was a risk undertaken by private investment, stabilizing a building that, given the low demand for office space, could have otherwise fallen into disrepair. This adaptive reuse, which included converting a level of structured parking to apartments, adding windows on an entire wing, created 103 residential units and housing for almost 300 people. Chick Fil A similarly will be investing privately and without economic incentive.

Uptown Rental Properties owns thousands of apartments and commercial space in the immediately adjacent area, and we would not be in favor of a use that is detrimental to the neighborhoods where our residents live, where our office is located and where we continue to invest. We respectfully urge you to approve the Planned Development as presented and designed.

Sincerely,

Patrice Burke Vice President, Real Estate Development



198 E. McMillan Street & 237 William H Taft Road Proposed PD District Program Statement

Addendum/Supplemental Information (4-1-2025)

Please consider the following additional information in support of the proposed PD District and Concept Plan for the Chick-fil-A at 198 E. McMillan Street and Union on Taft at 237 William H Taft Road. This information is being provided following discussion with the City Planning staff to provide an expanded explanation and support information for this project for consideration by City Planning Commission and City Council. We believe the following information responds to the decision criteria contained in the Cincinnati Zoning Code Section 1429.01 to Section 1429.11. (Specific responses are provided on pages 8 through 10 of this Addendum.)

Union Taft LLC is the owner of approximately 5.5 acres at the northwest corner of E. McMillan Street and Highland Avenue. The property currently contains a parking lot at 198 E. McMillan Street with approximately 145 parking spaces on 1.47 acres and Union on Taft at 237 William H Taft Road, a four story, 103-unit multifamily apartment building managed by Uptown Rental Properties on approximately 4 acres. The property is currently zoned OG-T Office General Transportation Corridor District.

We believe a Planned Development PD District is the appropriate zoning designation for the property for several reasons:

- 1. <u>Streamlined Decision Process.</u> Presenting an overall plan for the existing apartments and the new quick serve restaurant allows a straightforward and coordinated decision by Planning Commission and City Council. The current OG-T District zoning does not allow the proposed restaurant use, so a zone change is required. A PD District is appropriate because a zone change to a "conventional" zoning district would require applications for Hearing Examiner relief for a combination of conditional use, special exception and zoning variances after the zone change. While the PD District process has multiple steps, in this specific case it provides opportunities for better predictability for all parties the developer, the city decision makers, city staff, and the neighborhood. The alternative of a zone change followed by a Hearing Examiner process results in piecemeal decision making and unpredictable outcomes. We believe that all parties, particularly decision makers, benefit from being presented with the full scope and scale of the project, instead of one body determining a zone change and another body acting on site adjustments and zoning relief. The PD District provides this benefit.
- 2. <u>Unify and Integrate Sites and Uses.</u> This project involves an existing residential development the Union on Taft apartments and the Chick-fil-A quick serve restaurant use. There are several site design elements that need to be integrated and coordinated between the two uses.
 - a. The owner intends to subdivide the property into two lots a 4 acre lot for the Union on Taft apartments and a 1.47 acre lot for the Chick-fil-A. Both lots will be owned by Union Taft LLC, but the 1.47 acre lot will have a ground lease to Chickfil-A. Using the PD District allows the owner and the City to ensure both sites are coordinated now and in the future.

- b. There will be shared access between the two uses. The existing driveway on Highland Avenue currently provides access to the loading and trash compactor area for Union on Taft. This same driveway provides access to the existing parking lot. The PD Concept Plan has been designed so that the refuse collection/dumpster area for Chick-fil-A will be in the same area as the existing service areas for Union on Taft. This allows joint use of the existing driveway on Highland Avenue. A cross access easement will be provided to ensure access by both uses.
- c. A coordinated sign plan and wayfinding is proposed. A ground sign is proposed at the intersection of William H Taft Road and Highland Avenue. This sign will have a panel for the existing Union on Taft apartments and the proposed Chickfil-A. Using the PD District limits precedent for offsite signage given the common ownership and specific conditions of this project.

 We believe that the shared access to Highland Avenue, coordination of service areas, the easement, coordinated wayfinding, and future control of zoning for both sites by the property owner can best be integrated by using the PD District.
- 3. <u>Site Context Is Appropriate For The Use.</u> The proposed Chick-fil-A use and the Concept Plan design are appropriate for this property based on the site context. The following define the context of the site and should be considered when evaluating the proposed PD District and Concept Plan.
 - a. <u>Market Changes Effecting Zoning Patterns Context</u>. The site is currently zoned OG-T Office General Transportation Corridor District. The OG District reflects a historical trend of office development pattern in this part of Uptown. However, trends are starting to shift, as institutional and traditional office uses that dominated this part of Mt. Auburn/Uptown are transitioning to different uses. This makes the OG District less responsive to market trends, as office demand diminishes.



b. <u>Transportation Corridor Context</u>. The property is in the Transportation Corridor designation of the zoning code. This 'T' designation is for areas located along

major transportation corridors. McMillan Street and William H Taft Road are both major transportation corridors. This portion of McMillan Street is one way east bound with 4 lanes (plus a parking lane) and approximately 13,750 vehicles a day. William H Taft Road is one way west bound with 4 lanes and approximately 15,500 vehicles per day. These are significant volumes given that both streets have one way traffic. Highland Avenue is 4 lanes wide, 2 lanes in each direction, and approximately 5,400 vehicles a day.

We believe that the Transportation Corridor component is relevant and should be considered when evaluating the land use and zoning outcomes. The auto oriented context of the site makes this property suitable for Chick-fil-A which thrives in corridors with high customer counts.

c. <u>Highway On Ramp to I-71 Context</u>. East of the site, this portion of McMillan Street is the entrance to Interstate 71. The north bound, double lane I-71 entrance ramp is approximately 1,900 feet (.36 miles) east of the intersection of McMillan Street and Highland Avenue. Once through the intersection, vehicles are moving to the left lane to prepare to enter the Interstate. Just east of the intersection is the overpass/bridge over Burnett Avenue that passes under McMillan Street. McMillan Street is a high volume transportation corridor.

We believe McMillan Street is a commuter and transportation corridor and that the

proposed use is appropriate for these conditions.



d. Parking Lot Context. This property has been a parking lot for more than 60 years since the 1960s. It has approximately 145 existing parking spaces and is currently underutilized. The parking lot has a chain link fence along both street frontages with a narrow strip of grass behind the fence. The site is almost completely paved and contains several retaining walls supporting Union on Taft.

We believe the proposed Chick-fil-A is a significant improvement and investment when compared to the existing parking lot site characteristics.



1996



2015



2006



e. <u>Site Size Context</u>. At 1.47 acres, the site is large in context to the Mt. Auburn neighborhood. The property has approximately 335 feet of frontage on McMillan Street and 210 feet along Highland Avenue. The size is large enough to accommodate the parking and circulation needs of Chick-fil-A.

- We believe the site can accommodate the proposed Chick-fil-A and that the Concept Plan addresses existing site conditions. This property is and has been auto oriented for more than half a century.
- f. Surrounding Use Context. To the east of the proposed Chick-fil-A property across Highland Avenue is an existing Taco Bell restaurant with drive through service. The neighboring property to the west is a parking lot. The Union on Taft apartments are to the north and northwest surrounding the Chick-fil-A site. There is a grade change between the Union on Taft and the Chick-fil-A site the apartments are higher and there is a large yard/open area along the Highland

Avenue frontage of the Union on Taft property. There is a retaining wall adjacent to the sidewalk on Highland Avenue for the Union on Taft property. The existing character and context of the site and area is defined by the open parking lot and the high volume of vehicle traffic in the corridor.

We believe the proposed Concept Plan and PD District are consistent with the current development pattern in the area and will be compatible with the mixed-use nature of the neighborhood.



Highland Avenue Shared Driveway for Service

Retaining Walls and Service Areas



Union on Taft Service Area

Existing Parking – Looking South



View of Site Looking Northeast

Existing Taco Bell Site

g. <u>Existing Building Setback Context</u>. The proposed building location for the Chick-fil-A is consistent with the building setbacks of adjacent properties as illustrated by the image below.

We believe proposed building setbacks are consistent with the current development

pattern in the area and will be compatible with the neighborhood.



- h. Adjacent Ownership Context. The property potentially most impacted is the Union on Taft. The owner of Union on Taft is the owner of the property where the Chickfil-A construction is proposed. Union on Taft/Uptown Rental Properties believe that the Chick-fil-A will be an appropriate land use and compatible neighbor. Uptown Rental Properties has built many multi-family developments and projects in the Uptown area and throughout Cincinnati. Uptown Properties has a unique perspective and ability to evaluate site feasibility.
 - We believe Uptown Properties' decision in favor of the proposed development is a significant indicator of market trends and shows confidence that property values will not be negatively impacted.
- 4. <u>Community Benefit and Need.</u> Mt. Auburn as well as Corryville, Walnut Hills and the overall Uptown area will benefit from the proposed Chick-fil-A. Chick-fil-A is a top of market quick service restaurant with a strong reputation for quality food, service, property maintenance and community support. Chick-fil-A will have between 60 and 100 employees, plus additional construction jobs. The Uptown area is one of the largest employment and residential centers in the city, and the addition of this use will serve the community as a whole.
 - We believe that Chick-fil-A at this site provides an opportunity to serve Uptown employees, residents, students and visitors with quality food service options.
- 5. <u>Site Design For Vehicles and Pedestrians.</u> Providing zoning and a site design that accommodates both vehicular and pedestrian customers is important and necessary for this site. The site has been designed based on the context of the neighborhood and the anticipated customers. There is a high volume of automobile traffic in the area, but walkin pedestrian customers are also expected to be significant at this location, from the

neighborhood and the adjacent housing. Pedestrian access has been planned in the design. There will be sidewalks from both streets to the front of the store. Crosswalks will be installed in the parking area. The proposed decorative metal fencing along the street frontages at the corner around the drive through lanes separate vehicle and pedestrian circulation and improve the streetscape experience. A unique building design for both the drive through order point and pick up area enhance the street frontage and add mass to the building, while creating a unique design for this location. <u>Proposed Site Renderings</u> are attached to this Addendum.

We believe that the Chick-fil-A has been designed as both neighborhood and community oriented to serve both pedestrian and auto customers. We believe that serving pedestrian and vehicular customers are not exclusive endeavors. We believe that the site is designed to accommodate both customer groups, and that the conditions and context of the area support this approach.

- 6. Consistency with Adopted Plans. The Program Statement included a brief assessment of consistency with Plan Cincinnati. The City Planning Staff suggested that we conduct a deeper evaluation of Plan Cincinnati as well as the 1992 Mt. Auburn Community Plan. Attached are several references and excerpts from both planning documents that we believe demonstrate that the proposed PD is supported by plan recommendations and that a finding can be made that the PD request is consistent with adopted plans. The following provides an overview of these planning documents and how the proposed PD request is consistent with adopted plans.
 - a. 1992 Mount Auburn Community Plan. The 1992 Mt. Auburn Community Plan identifies the intersection of McMillan Street and Highland Avenue as an area where business clusters should be encouraged. The 1992 Plan has several goals related to the need for business to serve the neighborhood, employment opportunities and jobs, and the need for the full spectrum of quality residential uses, including student housing. The Plan recommends making the McMillan Street and Highland Avenue intersection a "secondary neighborhood business focus".
 - b. 2012 Plan Cincinnati. Plan Cincinnati makes few references to the Mt. Auburn neighborhood relative to Neighborhood Centers and community development. It does refer to Mt. Auburn as having one of the highest percentages of housing units without vehicles which could correlate to needing more services that are walkable within the neighborhood. The Geographic Principles points out that a portion of Mt. Auburn is underserved by a center of activity as it does not have its own neighborhood center. Plan Cincinnati lacks specific recommendations for Mt. Auburn, but we believe that the proposed PD District meets general recommendations from the Livability Principles, Compete Initiatives and other guiding policies by providing neighborhood oriented service, investment and job opportunities.

Decision Criteria in Cincinnati Zoning Code Section 1429.11

§ 1429-01. Specific Purposes.

(a) Establish a procedure for the development of land in order to allow for a more efficient and economic development of property than ordinarily permitted by conventional zoning and subdivision regulations.

The existing OG-T District does not allow the proposed use. Changing to another conventional zoning district (i.e. CC-M-T) would require a site design that does not allow the proposed use to function. We believe that the PD Planned Development District allows an efficient development of the property by allowing City Planning Commission and City Council to review and approve the development plan AS A WHOLE instead of utilizing a multi-stage process involving a zone change followed by Hearing Examiner action on conditional use, special exception, and variances. The PD District allows coordinated development of the subject property for two different uses, recognizing single ownership, coordinating site access, and common trash and service delivery areas. We believe that the site is carefully and uniquely designed and accommodates the high volume transportation corridor site context while also providing safe pedestrian access. We believe the building placement and overall site design are consistent with the site context and the vicinity.

(b) Ensure orderly and thorough planning and review procedures that lead to quality design and development.

We believe the PD District process allows detail review and negotiation by the city staff, Planning Commission, City Council and community input that results in design elements for the proposed Chickfil-A that are specific and unique for this property and context. The primary concern of the Mt. Auburn Community Council was addressing traffic congestion and access. The proposed plan, with the traffic study approved by DOTE, addresses the primary community concern. The PD District allows an outcome not feasible by the CC-M-T District that is supported by the City Staff.

(c) Encourage creativity in developments by allowing greater flexibility in access, light, open space and amenities.

The proposed Concept Plan provides shared access easements for the Union on Taft apartments and the Chick-fil-A restaurant, allowing trash compactor, loading and dumpster areas to be shared, and accounting for existing grade separation and retaining walls. The proposed plan maintains one point of access on Highland Avenue for the two uses, instead of creating separate driveways. Existing open space is maintained. A lighting plan has been submitted showing compliance with city standards.

(d) Encourage common open space and provide for its maintenance.

The proposed development proposes common open space and the owner will provide for its maintenance.

(e) Encourage the coordinated development of properties that might otherwise be developed individually, which may be a detriment to the surrounding neighborhoods and the developer.

The proposed development is coordinated as Uptown rental properties will maintain ownership of both phases.

§ 1429-11. City Planning Commission and Council Action.

- (a) City Planning Commission Action.
 - (1) The PD concept plan and development program statement are consistent with applicable plans and policies and is compatible with surrounding development;
 - We believe the proposed PD Concept Plan and Development Program Statement, with the Addendum dated April 1, 2025, are consistent with plans for Mt. Auburn, and that the project is compatible with surrounding development. We have provided an analysis of both 2012 Plan Cincinnati and the 1992 Mt. Auburn Community Plan that support the proposed plan.
 - (2) The PD concept plan and development program statement enhance the potential for superior urban design in comparison with the development under the base district regulations that would apply if the plan were not approved;
 - The context of this project is largely defined by the predominance of existing vehicular uses (parking lot and Taco drive through) and transportation corridors (multi lane, high volume, Interstate ramp access). But the site context also deserves planning for pedestrian access. We believe the proposed concept plan blends the strong vehicular oriented demand of the site with building design, pedestrian access, and streetscape themes that create an urban design appropriate for this site.
 - (3) Deviations from the base district regulations applicable to the property at the time of the PD application are justified by compensating benefits of the PD concept plan and development program statement; and
 - The PD District allows for coordination of access for service, trash compactor and dumpster locations between the two properties. The proposed dumpster service area for the proposed Chick-fil-A are located near the service areas for the Union on Taft, allowing shared access to both properties from Highland Avenue, preventing the need for separate driveways for the two uses. The PD Concept Plan also proposes a shared sign package, providing wayfinding for the proposed Chick-fil-A and Union on Taft apartments. Both properties benefit from the Concept Plan.
 - (4) The PD concept plan and development program statement includes adequate provisions for utility services, refuse collection, open space, landscaping, buffering, pedestrian circulation, traffic circulation, building design and building location.

The Concept Plan addresses these criteria as follows:

- The concept plan provides coordinated refuse collection as explained above.
- Utility services are available for both uses.
- Open yards are maintained around Union on Taft street frontages. Landscaping is proposed along the street frontages and in the parking areas for the Chick-fil-A. Sidewalks and crosswalks are proposed from both McMillan Street and Highland Avenue to the Chick-fil-A. There is currently a chain link fence that surrounds the existing parking lot. A 5 foot wide tree lawn adjacent to the curb, a 5 foot wide sidewalk and then a 5 foot wide landscape area on the site will be provided along the McMillan Street frontage. On the Highland Avenue side, a 7 foot wide sidewalk will be maintained and a 5 foot wide landscape area is provided. A decorative metal fence will

- be installed along both frontages at the corner around the proposed customer drive through area. The proposed streetscape landscaping and fencing define vehicular circulation areas and separate pedestrian paths creating a safe environment.
- A traffic impact study has been prepared and approved by DOTE. The traffic study determined that the existing lanes on Highland Avenue have enough capacity to allow left turns at the Highland Avenue Chick-fil-A driveway. The study finds no significant impact on Highland Avenue traffic. Queuing of southbound traffic will not block the Highland Avenue driveway. On McMillan Street, because the street is one way, there is no "opposing traffic" that impacts left turns on to Highland Avenue from McMillan Street or from the Chick-fil-A driveway onto McMillan Street. McMillan Street has four lanes available for movement and minimal delays are expected from left turning vehicles. Both driveways are recommended to have one lane in and one lane out with stop sign controls for exiting traffic.
- Regarding building and site design, a screen wall with metal canopy roof is proposed for the customer order point on the McMillan Street side. This design will have masonry columns and "window" openings. We believe this design is unique and will enhance the street presence of the building. The customer pickup point on the north side of the building will also have a covered roof and further define the proposed building massing. The building will be constructed of a combination of brick, pre-finished metal coping and storefront accents. Alternate site designs were evaluated; however, it was determined that drive through access on the north would eliminate driveway access from Highland Avenue which is necessary for both loading, trash service and customer access for both uses. See Proposed Site Renderings attached to this Addendum.

Review of 1992 Mount Auburn Community Plan

We believe the following excerpts and references from the 1992 Mount Auburn Community Plan are relevant to the proposed site and PD District.

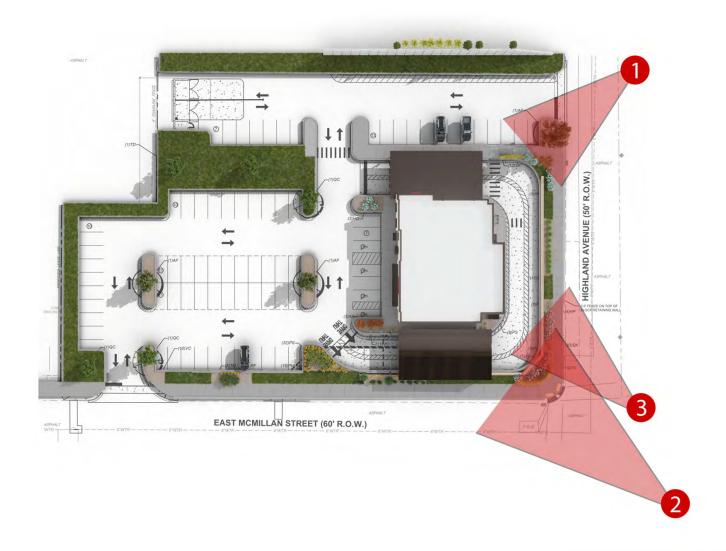
- Executive Summary. Establishing "clusters" of similar uses can enhance the land use patterns of Mt. Auburn. Businesses should be located at important "nodes" within the community.
- Page 6. Major issues to be addressed by the Mt. Auburn Community Plan include promoting job training and job creation in Mt. Auburn and throughout Uptown. Retain current residents and attract new residents.
- Page 10. Goals and Objective: Encourage quality housing.
- page 11. (d) Use rehabilitation as a primary tool to provide housing.
- page 28. B. McMillan Street is designated an Arterial through street. Highland Avenue is a local service street.
- Page 37. Goal: Make Mt. Auburn an attractive and livable community. (a) Upgrade vacant properties to assure adequate public safety and aesthetics. Goal: Establish the appropriate balance of residential uses and commercial/institutional uses needed to maintain the neighborhood's character.
- Page 38. (a) Encourage only those "region serving" uses which are compatible with the
 public peace, safety and nearby residential neighborhoods. Goal: Provide zoning to
 ensure appropriate type and mix of uses and ensure quality development which is
 compatible with the character of the neighborhood. (e) Make the character of the new
 development appropriate to the existing character.
- Page 41. Business clusters are located at various intersections, including Highland Avenue/McMillan Street. These clusters consist of business uses intermingled with vacant buildings and residential uses.
- Map 8, page 44. A. Shows the intersection of McMillan Street and Highland Avenue as professional office and business cluster.
- Page 45. Attention should be focused on preserving, improving and facilitating business
 development close to the existing business clusters. The McMillan Street/Highland
 Avenue intersection is located along transits, have the potential of being focal areas for
 business uses. Enhancing the existing strong business clusters can stimulate new
 businesses in Mt. Auburn. To reduce Mt. Auburn's unemployment rate, emphasis should
 be placed on commercial developments that provide temporary and permanent
 employment opportunities for Mt. Auburn residents.
- Page 46. Goal: Limit retail business development to centralized locations near transit and residences. (B) Couple proposed or existing retail establishments with other pedestrian oriented land uses to help attract a maximum number of residential consumers. (d) Make McMillan Street/Highland Avenue intersection a secondary neighborhood business focus. Goal: Encourage appropriate retail and selected services.
 (a) Seek businesses that can meet neighborhood needs and can be financially supported by the neighborhood.
- Page 47. Goal: Target unemployment to be no greater than the city's rate. (a) Encourage commercial development that generates jobs for community residents.

Review of 2012 Plan Cincinnati

We believe the following excerpts and references from the 2012 Plan Cincinnati are relevant to the proposed site and PD District.

- Livability Principles. #3. Enhance economic competitiveness. Improve economic competitiveness through reliable and timely access to employment centers, educational opportunities, services and other basic needs by workers, as well as expanded business access to markets.
- Guiding Policy Procedures. Increase our population. To do this we will be a vibrant city with a collection of "neighborhoods of choice", offering a full spectrum of housing options for people at all stages of life and welcoming and engaging public spaces.

 Union on Taft provides student housing, an important element for the support of the University of Cincinnati, but also a housing solution that helps maintain access to neighborhood housing in Mt. Auburn and surrounding Uptown communities to non-student residence.
- Build Our Assets. Our neighborhoods are structured around centers of activity that
 contain all of the amenities that we need to go about our daily life. We will focus our
 development on these centers of activity and strategically select areas of new growth.
- Guiding Geographic Principles. #1 Focus revitalization on existing centers of activity. Mt. Auburn is not a neighborhood center in Plan Cincinnati. The subject property is in a gap area between CUF, Corryville and Walnut Hills Activity Centers. But the 1992 Community Plan identifies the intersection as a business cluster area. While the area tends toward the automobile it is not in a defined neighborhood center that other neighborhoods have. We believe this supports the idea for the proposed use, with a design that recognizes pedestrian access and complements the streetscape.
- Mt. Auburn is not designated in one of the three neighborhood types- urban center, urban neighborhood, traditional neighborhood in Plan Cincinnati.
- Mt. Auburn is identified as a neighborhood with high percentages of housing with no vehicles available.
 - We believe this demonstrates the need to provide pedestrian access. But Mt. Auburn is also a community with high traffic volume corridors which tends towards auto oriented uses being appropriate.
- Compete Initiative. Businesses want to operate in places where they can attract and retain talented employees.... Our city's quality of life and livability must be our highest priority.
 - We believe Chick-fil-A is a support food service business that improves quality of life for residents, employees and commuters.
- Live Initiative, Goal 3. Provide a full spectrum of housing options and improve housing quality and affordability. Offer housing options of varied sizes and types for residents at all stages of life.
 - The Union on Taft apartments meets this goal and provides needed quality student housing. It increases the supply for other residents so that they can live in Mt. Auburn.





CORRYVIIIE, OH E MCMILLAN STREET & HIGHLAND AVE CINCINNATI, OHIO 45219

ALL COLORS AND MATERIALS REPRESENTED ARE A CLOSE APPROXIMATION AND SHOULD BE CONSIDERED VISUAL DESIGN INTENT ONLY. THIS DOCUMENT IS NOT TO BE USED FOR CONSTRUCTION, BID, RELIANCE OR IMPLEMENTAL





CHICK-FIL-A

CORRYVIIIE, ON
E MCMILLAN STREET & HIGHLAND AVE
CINCINNATI, OHIO 45219

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CHICK-FIL-A

CORRYVIIIE, OH
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March 31, 2025

City of Cincinnati
Department of Planning & Engagement
Two Centennial Plaza
805 Central Avenue, Suite 720
Cincinnati, OH 45202

Dear Planning Staff,

Uptown Rental Properties, represented by Patrice Burke, has presented the Chick Fil A (CFA)project to the Mt. Auburn Community Council (MACC) on several occasions and a summary of the outreach is below.

August 2024 – Ms. Burke presented a concept plan showing a CFA drive-thru at the vacant surface parking lot on the corner of Highland & McMillan which would require a zone change. The community expressed concerns about the volume of traffic that a CFA would generate in addition to the existing traffic at the intersection that can cause congestion, speeding, and running red lights. Some residents expressed that it is great idea to use the vacant parking lot and bring in business, and that other traffic concerns when the hotel at Reading Road was built have not come to fruition. MACC asked Ms. Burke to come back to the Council with more details about the traffic plan, as well as a representative from CFA to answer operational questions.

October 2024 – Ms. Burke attended with Ms. Shaun Walker, Principal Development Lead, Chick Fil A who gave a presentation via Zoom. Ms. Walker estimated that 60 to 120 people would be employed at this location, and she stated that CFA donates thousands of dollars annually to local charities and gives scholarships to associates. She stated that this site is a new prototype for CFA which has evolved over time to contain all traffic onsite. Residents had very specific questions about traffic, including the number of vehicles on E McMillan per day (estimated 20,000 from DOTE), projections for traffic into CFA (150-200 per hour at peak hours), how many

drive through lanes (2), how many entrances (2). Residents said that the traffic figures seem outdated and do not account for game days or other events. MACC asked Ms. Burke to have DOTE present the results of CFA's traffic study to the council.

February 2025 – Ms. Burke stated that this Chick Fil A is a unique location among Cincinnati locations because of the large size of the site, the multiple access points, and that it will not share a use with other retailers. She stated that traffic volume on Highland and E McMillan is less now that the MLK interchange is operational. She also stated that Chick Fil A had completed their traffic study and had made modifications to the site, including increasing the size of the sidewalks, adding landscaping, a fence and wall, and a covered structure at the drive through that goes to the landscape buffer. Bryan Williams of DOTE presented the traffic findings at the site and answered questions. Mr. Williams reviewed the study and agreed with the study that modifications to the Right of Way do not need to be made. CFA has increased sidewalk and drive apron to help pedestrian safety. Neighbors think there will be an issue with congestion northbound on Highland as it is a road with four narrow lanes. MACC members expressed concern, as Highland Ave. is not treated as a four-lane street by drivers. In addition, there is legal parking on this street. MACC members agree that there should not be access to Chik Fil A for traffic driving north on Highland, as it will cause traffic issues. Mr. Williams stated that exiting could be an issue at times but that is CFA's issue to deal with onsite. Kyle Gibbs with Cincinnati Planning & Engagement was also present and explained the zoning process. Ms. Burke said that she would forward the Traffic Impact Study to the MACC.

February (several times) – Ms. Burke emailed the group offering to tour onsite at various times over the course of two weeks. Several members of the council toured onsite at the proposed location during peak traffic times and those who visited the site agreed that it was helpful to see in context and that they were not as concerned with traffic on McMillan since it is a 4-lane, one-way street. They also expressed that they liked the location of the curb cut on McMillan at a far distance from the intersection which is difficult to see in plan. Ms. Burke sent a video to the MACC of the site to circulate to members.

March 6, 2025 -There was a public staff conference hosted by Kyle Gibbs of Planning which Ms. Burke and several MACC members attended. Traffic flow on Highland was identified as a concern.

Ms. Burke has agreed to continue to engage with MACC throughout the process, including through construction and as CFA operates.

Sincerely,

Lee Wilson, MACC President

205 WHT LLC	319 HILTON LLC	AMI WALNUT HILLS VISION LLC
12 SURREY DR	9638 E POWER DR	24 E UNIVERSITY AVE OFC
BROOKFIELD CT 06804	ENGLEWOOD CO 80111	CINCINNATI OH 45219
ANNA LOUISE INN	ARGAZ LLC	AVA RENOVATION LLC
300 LYTLE ST	24 E UNIVERSITY AVE STE #OFC	6721 KENWOOD RD
CINCINNATI OH 45202	CINCINNATI OH 45219	CINCINNATI OH 45243
BAKER JOSEPH & GERILYNN 8924 WINTERGREEN DR UNIT 201 WEST CHESTER OH 45069	CINCINATI BOARD OF EDUCATION PO BOX 5384 CINCINNATI OH 45201-5384	BUSHWOOD CAPITAL II LLC P O BOX 42602 CINCINNATI OH 45242
COMMERCIAL PROPERTIES USA LLC 1320 NAGEL RD #814 CINCINNATI OH 45254	COOPER EMMETT G SR MD PHD 6890 BEECHLANDS DR CINCINNATI OH 45237	CRAYON DENISE M 2447 MAPLEWOOD AVE CINCINNATI OH 45219
CTI PROPERTIES LLC	DOGTOWN 2519 LLC	EDEN CORRY LLC
PO BOX 1757	2519 BURNET AVE	2616 SHORT VINE
WEST CHESTER OH 45069	CINCINNATI OH 45219	CINCINNATI OH 45219
EKO PROPERTIES OHIO LLC	ELLIOTT REAL ESTATE LLC	EQUITY TRUST COMPANY
10901 REED HARTMAN HWY #316	5685 CHESTNUT RIDGE	8358 WOODBINE AVE
CINCINNATI OH 45242	CINCINNATI OH 45219	CINCINNATI OH 45216
EVANS RYAN & ELAINE KAO 4429 NORTH ST DALLAS TX 75214	FIRST CHRISTIAN ASSEMBLY OF GOD INC 220 WILLIAM HOWARD TAFT CINCINNATI OH 45219	GANAPATHI LLC 8616 ARROWWOOD DR MASON OH 45040
GEORGIEVA ANGELINA & PANDO GEORGIEV 915 NE 12TH AVE GAINESVILLE FL 32601	GLOBAL MEDICAL SERVICES LLC 173 E MCMILLAN ST CINCINNATI OH 45219	HAMILTON COUNTY BOARD OF COMMRS 138 E COURT ST ROOM 603 CINCINNATI OH 45202
HARRIS PAMELA GRIFFITH & STEVEN D 2481 PARIS ST CINCINNATI OH 45219-2711	HIGGINS KELLY III 153 EAST MCMILLAN AVE CINCINNATI OH 45219	HIGHOAK PROPERTIES LLC 2340 VICTORY PKW STE 41 CINCINNATI OH 45206
HOME SOLUTIONS FOR ALL LLC	HR HOLDINGS I LLC	IRBY VICKIE A & WILLIAM THOMAS
1320 NAGEL RD # 814	1418 CENTRAL PKWY # 201	2613 BELLEVUE AVE
CINCINNATI OH 45254	CINCINNATI OH 45202	CINCINNATI OH 45219

JOHNSON RACHETTA T	KMC UNLIMITED LLC	MCDOWELL DIANE S
2489 PARIS ST	2422 SYLMAR CT	2434 MAPLEWOOD AVE
CINCINNATI OH 45219	CINCINNATI OH 45233	CINCINNATI OH 45219
MCKENZIE LINDA R	MOON KIMBERLY F	NB CP CINCY DST TRUST
2470 PARIS ST	2451 MAPLEWOOD AVE 2ND FL	20 ENTERPRISE #400
CINCINNATI OH 45219	CINCINNATI OH 45219	ALISO VIEJO CA 92656
NEW LIFE PROPERTIES INC 401 E MCMILLAN ST CINCINNATI OH 45206	NFTL LLC 7737 GLEN EDEN LN CINCINNATI OH 45244	ORI ASSOCIATES V LLC 119 WEST CENTRAL PKWY STE 201 CINCINNATI OH 45202
PATEL HITESH & ROSHNI PATEL	PEOPLES CHURCH CINCINNATI	PEOPLES CHURCH CINCINNATI
130 E WALNUT ST	2602 EDEN AVE	220 WILLIAM HOWARD TAFT RD
PAINESVILLE OH 44077	CINCINNATI OH 45206	CINCINNATI OH 45219
POWELL RHONDA	RED DOOR MW LLC	S AND S PROPERTIES LLC
2430 HIGHLAND AVE	4414 GROVE AVE	5056 MEYERS LN
CINCINNATI OH 45219	CINCINNATI OH 45227	CINCINNATI OH 45242
SB INVESTORS LLC 3013 NORTH BEND RD CINCINNATI OH 45239	SHELTER HOUSE VOLUNTEER GROUP 1410 RACE ST CINCINNATI OH 45202	SORENSEN CHRISTINE 3980 KENKEL AVE CINCINNATI OH 45211
SORENSEN DIANE 4311 BEACH HAVEN CT DAYTON OH 45424	STATE OF OHIO OFFICE OF GENERAL COUNSEL PO BOX 210623 CINCINNATI OH 45221-0623	TAYLOR TAU REAN 2455 MAPLEWOOD AVE CINCINNATI OH 45219
THE CHRIST HOSPITAL	UC TOWER LLC	WALLACE REAL ESTATE LLC
2139 AUBURN AVE	2563 15TH ST STE 104	44 WEST FREEDOM WAY UNIT 204
CINCINNATI OH 45219	DENVER CO 80211	CINCINNATI OH 45202
WYDE JENNA ASHLEY 16800 E BAILS PL AURORA CO 80017	XU MIN 3816 HUDSON HILLS LN MASON OH 45040	MT. AUBURN COMMUNITY COUNCIL P.O. BOX 19138 CINCINNATI OH 45219
CORRYVILLE COMMUNITY COUNCIL 3211 GLENDORA AVE CINCINNATI OH 45220	UNION ON TAFT LLC 2718 SHORT VINE ST CINCINNATI OH 45219	



April 16, 2025

Cincinnati City Council Council Chambers, City Hall Cincinnati, Ohio 45202

Dear Members of Council:

We are transmitting herewith an Ordinance captioned as follows:

AMENDING the official zoning map of the City of Cincinnati to rezone the real property located at 198 E. McMillan Street and 237 William H. Taft Road in the Mt. Auburn neighborhood from the OG-T, "Office General-Transportation," zoning district to Planned Development District No. 104, "Chick-fil-A".

Summary:

The petitioner, Union on Taft LLC, has requested a Zone Change from Office General – Transportation Corridor (OG-T) to Planned Development No. 104 (PD-104) to facilitate a mixed-used development on the property. The Planned Development will include an existing multifamily apartment building and a new restaurant with drive-through service. The site is within the Mt. Auburn neighborhood.

If approved, the proposed mixed-use development would include 103-units of student housing with 180 parking spaces and an outdoor courtyard, and a new Chick-fil-A restaurant with 76 parking spaces, a two-lane drive-though, and seating for 94 guests. The new restaurant is proposed to be a standalone building, approximately 5,020 square feet and 21 feet tall. The proposal includes pedestrian walkways connecting to adjacent sidewalks with internal crosswalks connecting the walkways to the building's entrance.

The City Planning Commission recommended the following on April 4, 2025, to City Council:

APPROVE the Concept Plan and Development Program Statement as submitted; and

ADOPT the proposed zone change from Office General-Transportation (OG-T) to Planned Development (PD) at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn.

Motion to Approve: Ms. Kearney Ayes: Ms. Beltran

Mr. Dansby

Seconded: Mr. Weber Mr. Eby

Ms. Kearney Mr. Samad Mr. Weber

THE CITY PLANNING COMMISSION

Katherine Keough-Jurs, FAICP, Director

Department of City Planning & Engagement



May 6, 2025

TO: Members of the Equitable Growth & Housing Committee

202500932

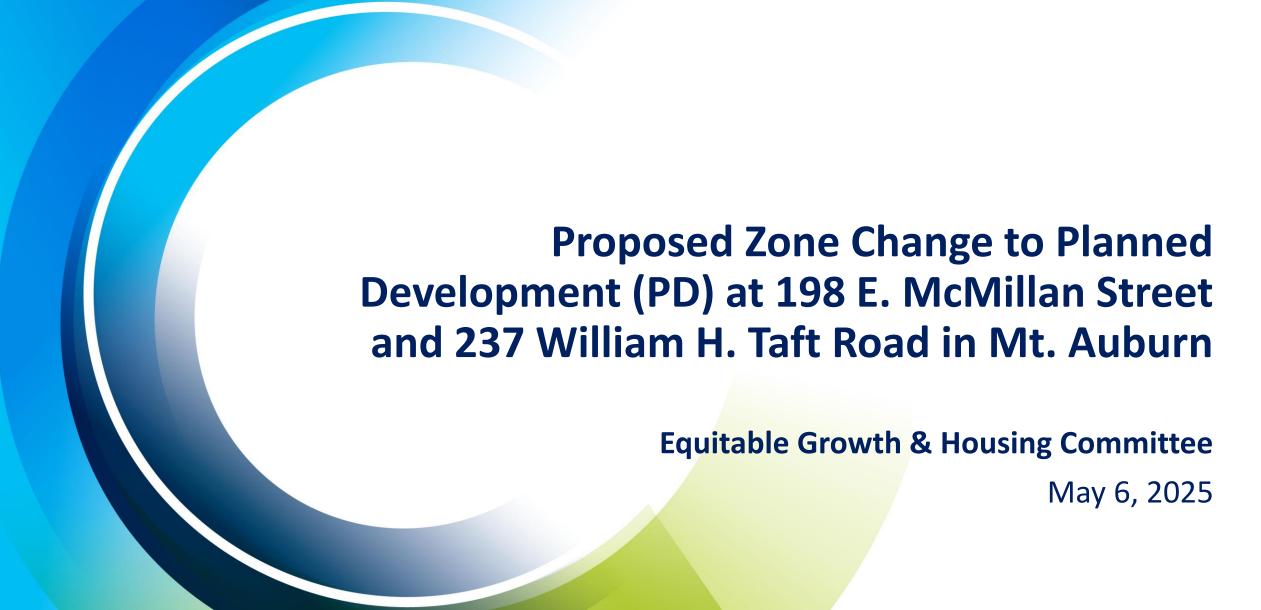
FROM: Sheryl M. M. Long, City Manager

SUBJECT: Presentation – Ordinance for the Rezoning of 198 E. McMillan Street and

237 William H Taft Road in Mt. Auburn.

Attached is the presentation for a Zone Change from Office General-Transportation Corridor (OGT) to Planned Development (PD), including a Concept Plan and Development Program Statement, at 198 E. McMillan Street and 237 William H Taft Road in Mt. Auburn to facilitate a mixed-use development including an existing apartment building and new Chick-fil-A restaurant.

cc: Katherine Keough-Jurs, FAICP, Director
Department of City Planning and Engagement



APPLICANT & OWNER

• Union on Taft, LLC

DEVELOPER

Uptown Rental Properties & Chick-fil-A Inc.

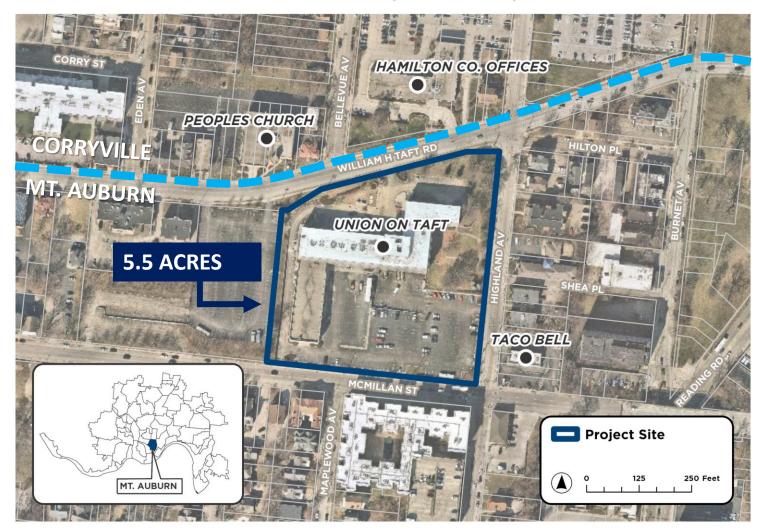
REQUEST

- Zone Change to Planned Development
- Allow for a mixed-use development with an existing multi-family apartment building and a new Chick-fil-A restaurant.





A PROPOSED ZONE CHANGE TO PLANNED DEVELOPMENT (PD) AT WILLIAM HOWARD TAFT, HIGHLAND, & MCMILLAN



NEIGHBORHOODS:

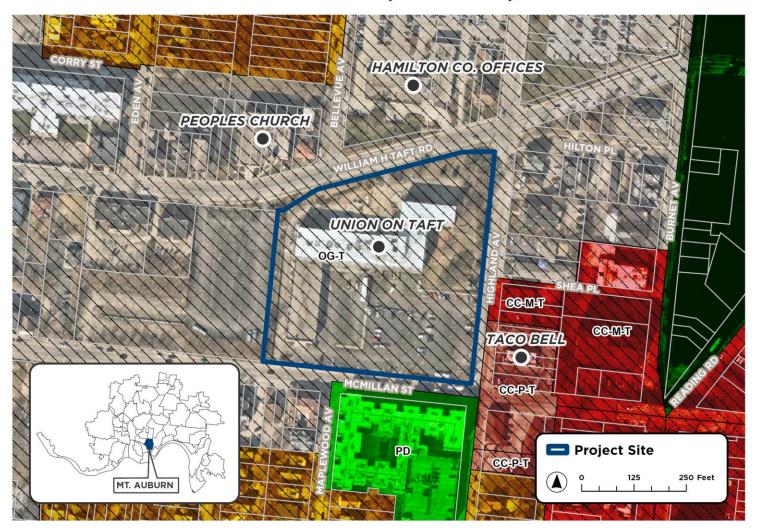
- Mt. Auburn
- Corryville (Adjacent)

SITE:

- 5.5 acres
- Existing Apartment Building
- 325 parking spaces



A PROPOSED ZONE CHANGE TO PLANNED DEVELOPMENT (PD) AT WILLIAM HOWARD TAFT, HIGHLAND, & MCMILLAN



SITE ZONING:

 Office General-Transportation Corridor (OG-T)

ADJACENT ZONING:

- OG-T
- CC-M-T
- CC-P-T
- PD



PD PURPOSE

§1429-01 – Specific Purposes of a Planned Development

- a. Establish a procedure for the development of land in order to allow for a more efficient and economic development of property than ordinarily permitted by conventional zoning.
- b. Ensure orderly and thorough planning and review procedures that **lead to quality design and development**.
- c. Encourage **creativity** in developments by allowing greater flexibility in **access**, **light, open space and amenities**.
- d. Encourage common open space and provide for its maintenance.
- e. Encourage the **coordinated development** of properties that might otherwise be developed individually.

PROPOSED DEVELOPMENT



USES

Phase One: Residential

Phase Two: Restaurant

LAND AREA

Phase One: 4 acres

• Phase Two: 1.5 acres

The two phases would be subdivided, if approved

PROPOSED DEVELOPMENT



PHASE ONE

- No proposed changes
- Completed in 2023
- Existing apartment building (The Union on Taft Apartments)
- 103-units (student housing)
- 180 parking spaces
- Outdoor courtyard





PROPOSED DEVELOPMENT

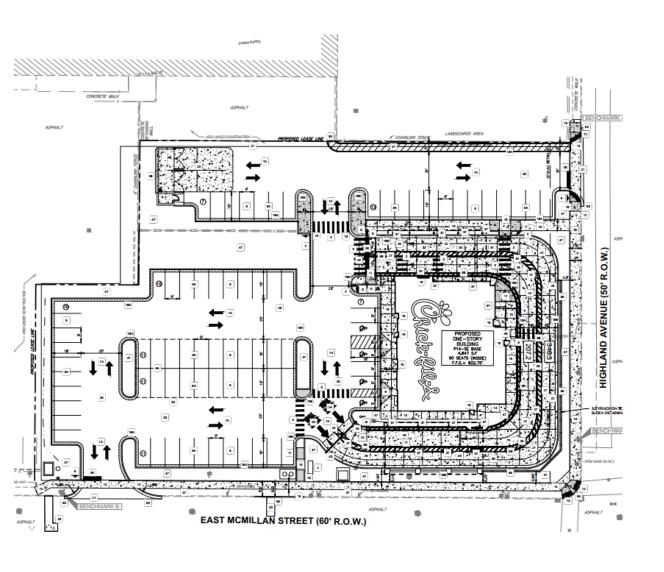


PHASE TWO

- Existing surface parking
- Proposed Chick-fil-A

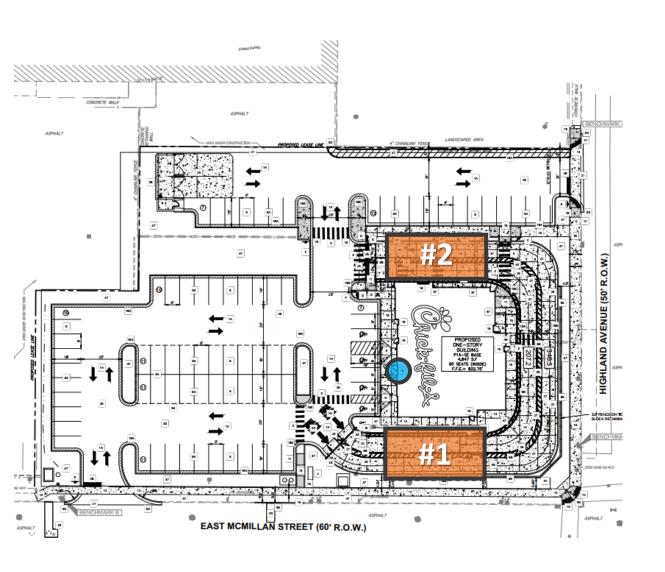






PHASE TWO

- One-story building
- Approx. 21-ft tall
- 5,020 square feet
- 76 Parking Spaces

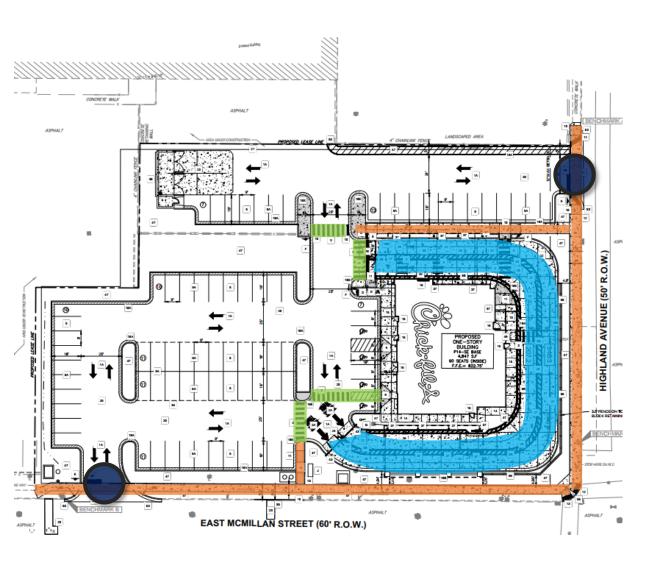


PHASE TWO

- Main entrance is on the west façade (blue dot)
- Two canopies are proposed (orange blocks)

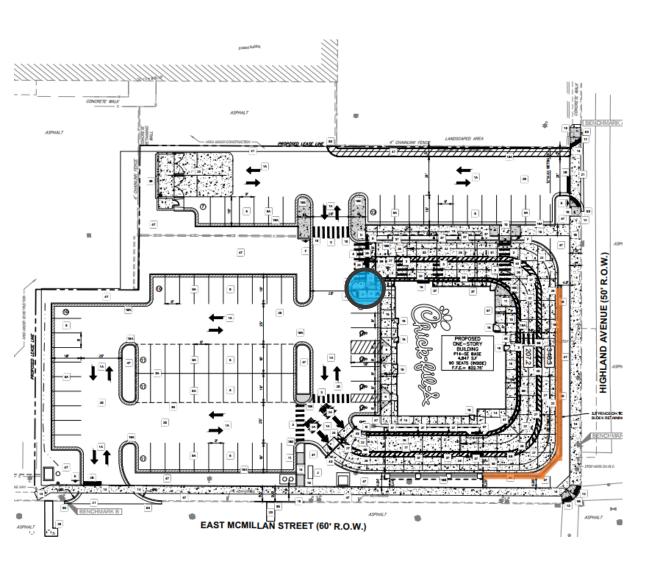
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#1 – Drive-through order point, approx. 60-ft long
```

#2 – Drive-through pick-up window, approx. 31-ft x 50-ft



PHASE TWO

- Two Entrances (blue dots)
- Two-lane drive-through (blue line)
- Pedestrian Network
 - Sidewalk access on E. McMillan Street and Highland Avenue (orange lines)
 - Four pedestrian crosswalks (green blocks)



PHASE TWO

- Outdoor eating area for four guests (blue dot)
- Existing fences and retaining walls would remain, except for a 20-ft reduction to one internal wall
- New proposed 2-ft retaining wall with 3.5-ft fence along Highland Avenue and E. McMillan Street (orange line)



BACKGROUND

Zoning Review			
Zoning District	OG-T (Existing Zoning District)	CC-P-T (Adjacent Commercial District)	CC-M-T (Adjacent Commercial District)
Uses Allowed	Restaurant use permitted with conditions Drive-throughs not permitted	Restaurant use permitted Drive-throughs not permitted	Restaurant use permitted Drive-throughs permitted in rear yard

COORDINATION

DCPE-Applicant Meetings

- (5) meetings throughout planning process
- Staff recommended changes to the site design to better comply with CC-M-T.
- Changes could not be made to preserve the optimal traffic flow required for the restaurant use.
- A zone change to Planned Development, that included the existing multi-family apartment building, was the only path forward.

PUBLIC ENGAGEMENT

Mt. Auburn Community Council (MACC)

- Applicant team has attended (5+) MACC meetings
- General support for the restaurant use
- Concerns about traffic
- MACC has not voted on support

Other community meetings

- Mt. Auburn CDC
- Corryville CDC
- University of Cincinnati
- The Port of Greater Cincinnati



PUBLIC COMMENT & NOTICE

- Public Staff Conference: March 6, 2025
 - (3) public members in attendance; (1) member of MACC
 - General support for the project
 - Praise for the applicant's involvement with MACC
 - Concerns around traffic flow on Highland Avenue and internal stacking spaces
- **CPC Notice:** March 21, 2025
 - Notices were sent to Mt. Auburn and Corryville Community Councils and property owners within 400-ft.
 - 1 public letter of opposition
 - Concerns about the site design and auto-oriented use of the site.



SITE UPDATES

Site Design Updates

- Updated Landscaping to comply with DOTE requirements
- New masonry wall and fencing along E. McMillan Street and Highland Avenue.
- New canopy along E. McMillan Street to bring the structure to the sidewalk and buffer vehicles.
- Improved pedestrian network:
 - Pedestrian walkway from Highland Avenue
 - Pedestrian walkway from E. McMillan Street
 - Internal crosswalks connecting the walkways to the building



TRAFFIC ANALYSIS

- **DOTE Accepted Traffic Analysis:** February 2025
 - DOTE accepted the analysis
 recommendations and determined that no
 changes are necessary for the right-of-way
 to support the traffic for the proposed
 restaurant.
 - Submitted by SHA Engineering, LLC

TRAFFIC ANALYSIS REPORT

FOR PROPOSED CHICK-FIL-A

HIGHLAND AVENUE, CINCINNATI, OHIO

Prepared For: Woolpert 4454 Idea Center Boulevard Dayton, OH 45430

Prepared By: Jamal Adhami, PE, PTOE SHA Engineering, LLC January 30, 2025 Updated February 20, 2025





Consistency with City Plans

- Plan Cincinnati (2012)
- Green Cincinnati (2023)
- Mt. Auburn Community Plan (1992)





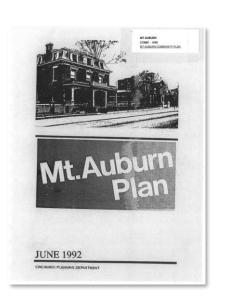


Consistency with City Plans

- Plan Cincinnati (2012)
- Green Cincinnati (2023) → Consistent
- Mt. Auburn Community Plan (1992) -> Consistent







Consistency with City Plans

- Plan Cincinnati (2012) → Both Consistent & Inconsistent
- Green Cincinnati (2023) → Consistent
- Mt. Auburn Community Plan (1992) -> Consistent







Planning Commission Action (§ 1429-11.a)

- PD plans are consistent with applicable plans and policies and is compatible with surrounding development;
- 2. PD plans enhance the **potential for superior urban design** compared to the applicable base zoning district;
- Deviations from the applicable base zoning district regulations are justified by the benefits of the PD design plans;
- 4. PD plans include **adequate provisions** for utility, trash, landscaping, traffic circulation, and other maintenance of the property.

PLANNING COMMISSION ANALYSIS

- The City Planning Commission supports the proposed zone change for the following reasons:
 - Site design and drive-thru location are justified based on the site's surroundings and location near I-71 and the traffic counts along
 E. McMillan Street.
 - Adjustments were made to the site design, including a pedestrian network and building edge along E. McMillan Street.
 - Job and business creation.
 - The applicant's **experience** with Uptown's market.
 - The applicant's interest in developing housing on the adjacent site.
 - **DOTE support** for traffic analysis.



RECOMMENDATION

The City Planning Commission recommends that City Council take the following actions:

- 1. ACCEPT the Concept Plan and Development Program Statement as submitted;
- 2. APPROVE the proposed zone change from Office General-Transportation Corridor (OG-T) to Planned Development (PD) at 198 E. McMillan Street and 237 William H. Taft Road in Mt. Auburn.